

. Frederick Avenue Corridor Master Plan .

FINAL REPORT

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Frederick Avenue Corridor Study

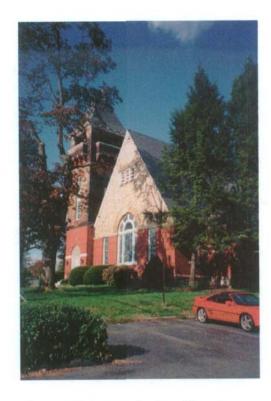
The Frederick Avenue Corridor Study is the result of a recommendation made in the City's Master Plan. Specifically, the Land Use element of the City's Master Plan recommended that a special study be conducted on the Frederick Avenue Corridor from Cedar Avenue to Montgomery Village Avenue. The study was to focus on the development of a comprehensive approach to improve the aesthetic quality of the Corridor, provide a safer pedestrian environment, and to ensure continued economic revitalization along the Corridor. The scope of the special study area was expanded to incorporate the entire length of the Frederick Avenue between Shady Grove Road in the south and Game Preserve Road in the north.

Frederick Avenue is a successful transportation and business corridor within the City of Gaithersburg, Maryland. Over 50,000 vehicles use the Corridor every weekday, either traveling to destinations along the Corridor or commuting to areas outside of the city. Frederick Avenue is struggling with many of the same issues being confronted by transportation corridors throughout America. Generally, these issues stem from uncontrolled and unplanned development as urban populations grow and spread beyond their metropolitan centers. As a result of similar development pressures, Frederick Avenue exhibits a cluttered and unpredictable building pattern comprised of a wide range of land uses including residential, commercial, office, service, hotels, churches, civic uses, parking lots, and public open space. Irregular building setbacks create a disorderly street edge. Signage and lighting standards are inconsistent, as are streetscapes and roadway standards. Pedestrian and bicycle traffic are not integrated into the overall street design, creating numerous difficulties for those using the Corridor. This Frederick Avenue Corridor Study addresses all of these issues.

The Frederick Avenue Corridor Study acknowledges the issues and challenges of the Corridor and presents ideas that will affect the appearance of the Corridor in a positive manner, realize the Corridor's redevelopment potential, and improve the quality of life along the Corridor. The report is organized into four main sections. The four sections include:

Master Plan Design Code The Phasing Plan Design Code Application

In summary, the Frederick Avenue Corridor Study provides a blueprint for the successful redevelopment of this four-mile segment of Frederick Avenue through Gaithersburg, Maryland.



Grace United Methodist Church

THE MASTER PLAN

General Notes

The Master Plan addresses the entire four-mile segment of Frederick Avenue. To develop an understanding of this four-mile segment of Frederick Avenue, the Corridor was divided into three distinct districts. The districts are based on the existing character and predominant land use pattern depicted in the geographic areas. The three districts are referenced on the Key Plan and are as follows: Southern Residential District, Fairgrounds Commercial District, Northern Employment District.

The Southern Residential District is characterized by the predominance of residential landuse patterns. There are single family homes, multi-family dwellings, three churches, and scattered retail fronting the Corridor. The retail is a mix of strip malls and transformed single family homes. The existing landscape is one of mature street trees, rolling topography, and limited building edge frontage. The Muddy Branch Stream is the predominant natural landform.

The Fairgrounds Commercial District extends from South Summit Avenue to Montgomery Village Avenue. The character of this area is determined by the commercial businesses that front along Frederick Avenue. The buildings are closer to the road and begin to form a built edge. There are several neighborhoods one building layer back from the commercial uses. The Montgomery Agricultural Center/Fairgrounds enhances the area and is a draw for tourists and local businesses.

The Northern Employment District is comprised of a variety of business uses ranging from large scale retail to automobile dealerships to hotels and large office buildings. The built edge irregularly spaced with set backs farther from the right-of-way.

The Master Plan describes the recommendations for physical improvements and redevelopment opportunities within each district. The recommendations vary among the districts in an effort to respond to existing characteristics and to impart to each district its own character that supports its purpose and function within the overall corridor and community. The recommendations are organized into two types of recommendations (or initiatives). The first is the Public Initiatives or those recommendations to be implemented by the City, and the second is the Private Initiatives or those recommended to be implemented by private development.

THE DESIGN CODE

General Notes

The Design Code was conceived as a guide for the redevelopment of the Frederick Avenue Corridor in Gaithersburg, Maryland. The provisions of the Design Code are derived from the vernacular architectural and planning traditions of the region. These have been modified to incorporate technical, social, and environmental progress. The Design Code is organized to respond to the three districts, identified in the Master Plan section of this study, with the intention of addressing the distinct characteristics of each district.

The Design Code applies to landuse, the design of private buildings and the design of public spaces. The Design Code will be enforced by the City of Gaithersburg. Public and civic buildings are exempt from the provisions of this code; their design will be negotiated with the City.

Specific waivers to the Design Code may be granted on the basis of unusual programmatic requirements, peculiar site constraints, hardship, or architectural merit as determined by the City.

Local building codes shall take precedence over the provisions of this Design Code.

The Design Code for each of the three districts consists of the following six interrelated documents:

The Regulating Plan
The Urban Regulations
The Architectural Regulations
The Landscape Architectural Regulations
The Lighting Regulations
The Environmental Graphic Regulations

The Regulating Plan: A series of drawings that identifies land use, frontage lines, and focal points.

The Urban Regulations: Text and drawings regulating those aspects of private development which affect the public realm. The Urban Regulations vary in response to the three districts. They define the streetscape and encourage the use of building elements that positively influence social behavior, such as shopfronts and awnings, stoops and porches.

The Architectural Regulations: Text that specifies the materials and configurations permitted for walls, attachments, roofs, openings, and storefronts. The Architectural Regulations are intended to produce visual compatibility among disparate building types. They relate the new buildings to the building traditions of the region.

The Landscape Architectural Regulations: Text that describes design treatments based on the three districts. They establish street tree spacing guidelines, site furniture guidelines, property line landscaping treatments, sidewalk treatments, special area treatments and recommended plant species. The landscape standards also highlight special areas such as gateways for more intensified treatments. Planting guidelines are one

means to reinforce and enhance the character of each zone, as they shift from urban to rural and from district to district.

The Environmental Graphics Regulations: Text and diagrams that specify size, type, and location of signage along the corridor. They establish guidelines for public and private signs that will enhance the visual environment, reduce the visual clutter, and make way-finding clearer and easier.

The Lighting Regulations: Text that specifies size, type, and location of both public and private lighting. The intent is to use lighting to create a safer and more attractive environment while conserving energy.

PHASINGPLAN

General Notes

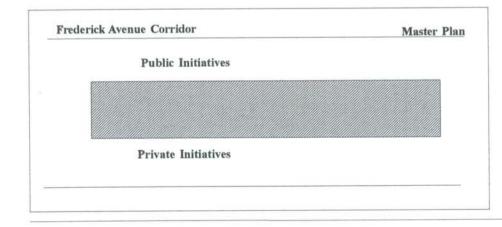
The Phasing Plan prioritizes the recommendations and projects identified in the Master Plan. The Phasing Plan addresses the recommended public initiatives and projects, and organizes them into seven phases along the four-mile corridor.

Phase One recommendations are illustrated and described in greater detail.

DESIGN CODE APPLICATION

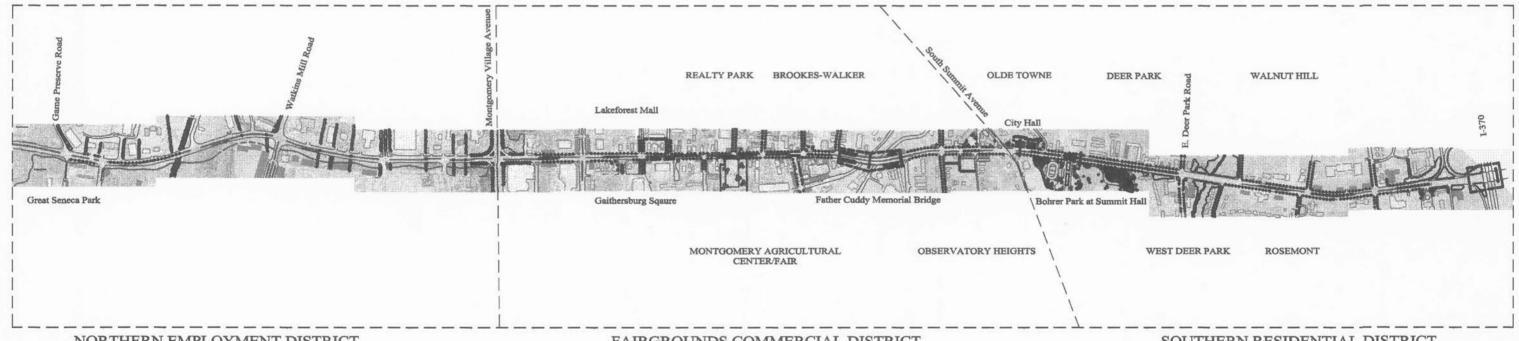
General Notes

The Design Code Application is a series of sketches that graphically depict the application of the Design Code to particular streetscape, architectural, neighborhood, lighting, signage and site furnishing circumstances within the Corridor.





View looking South from Education Boulevard.

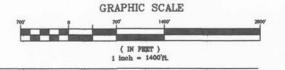


NORTHERN EMPLOYMENT DISTRICT

FAIRGROUNDS COMMERCIAL DISTRICT

SOUTHERN RESIDENTIAL DISTRICT



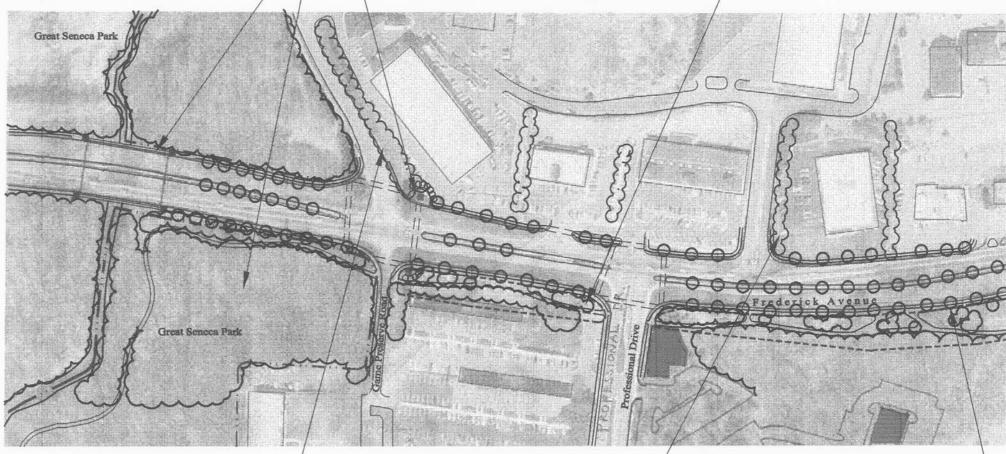


Add proposed City Entry Markers to both sides of Frederick Avenue. Provide additional landscape backdrop to help screen adjacent buildings.

Annex portion of Great Seneca Park from center line of Great Seneca Creek to Game Preserve Road.
Rejuvenate Great Seneca Bridge to provide gateway into the City.
Treatment should include new lights, piers and cladding. See Landscape

Architecture Regulations.

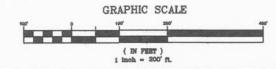
Establish Frederick Avenue Linear Park. Park easement should be from 100' to 200' in width along the western edge of Frederick Avenue. See Landscape Architectural Regulations.

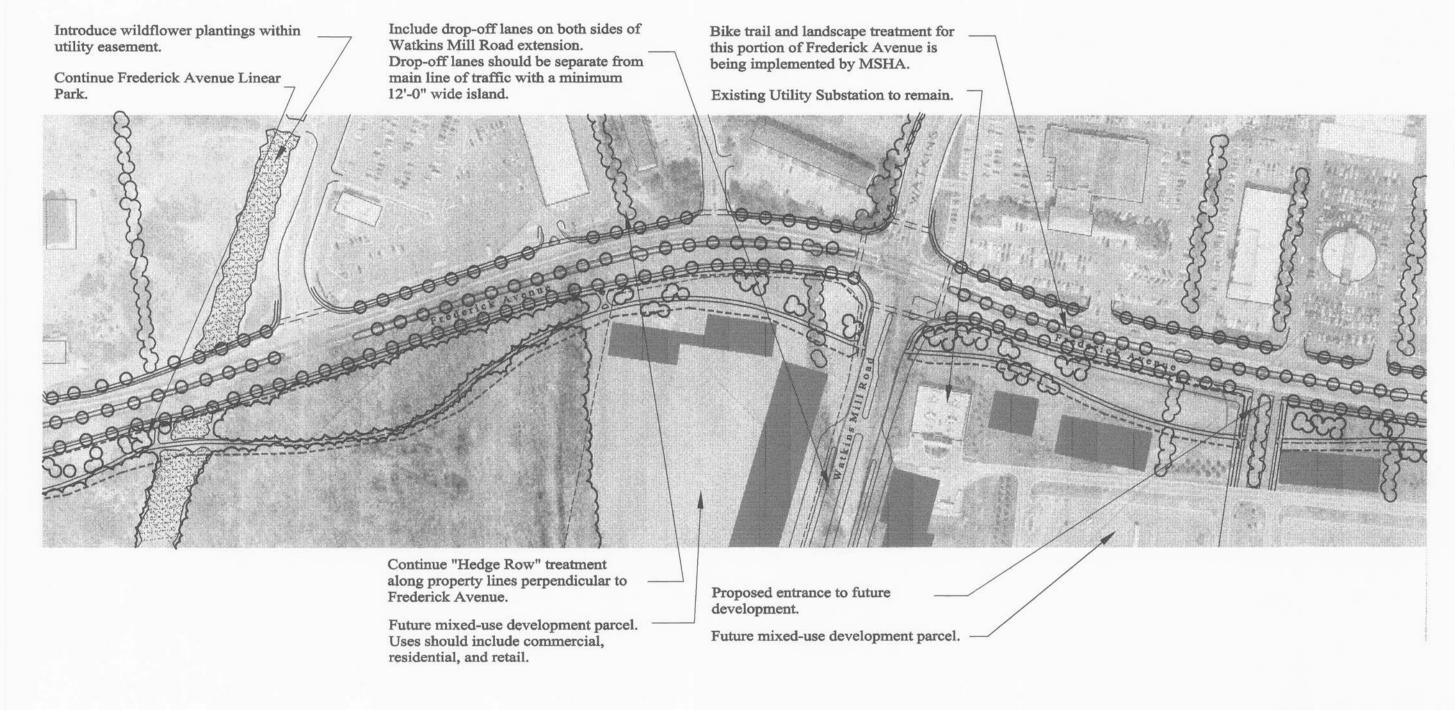


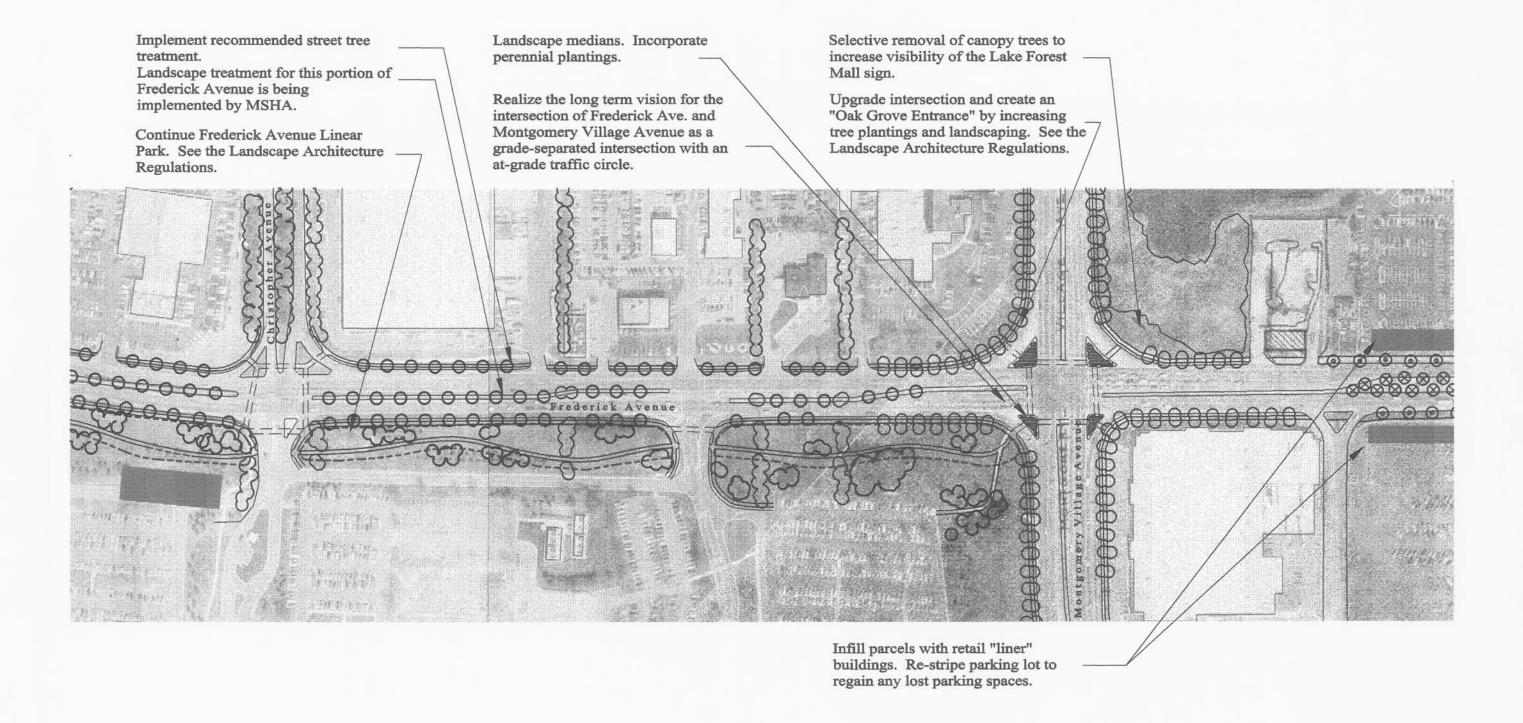
Provide additional landscape screening with a mixture of evergreen and deciduous trees along Game Preserve Road.

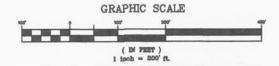
Incorporate "Hedge Row" treatment along property lines perpendicular to Frederick Avenue. Placement and selection of species should be made to maintain view to existing businesses along Frederick Avenue.

Bike trail and right-of-way landscape treatment in this area is being implemented by Maryland State Highway.









Implement recommended street tree treatment.

Remove the existing concrete medians and their plantings. Add new planting beds with deciduous trees. See Landscape Architecture Regulations.

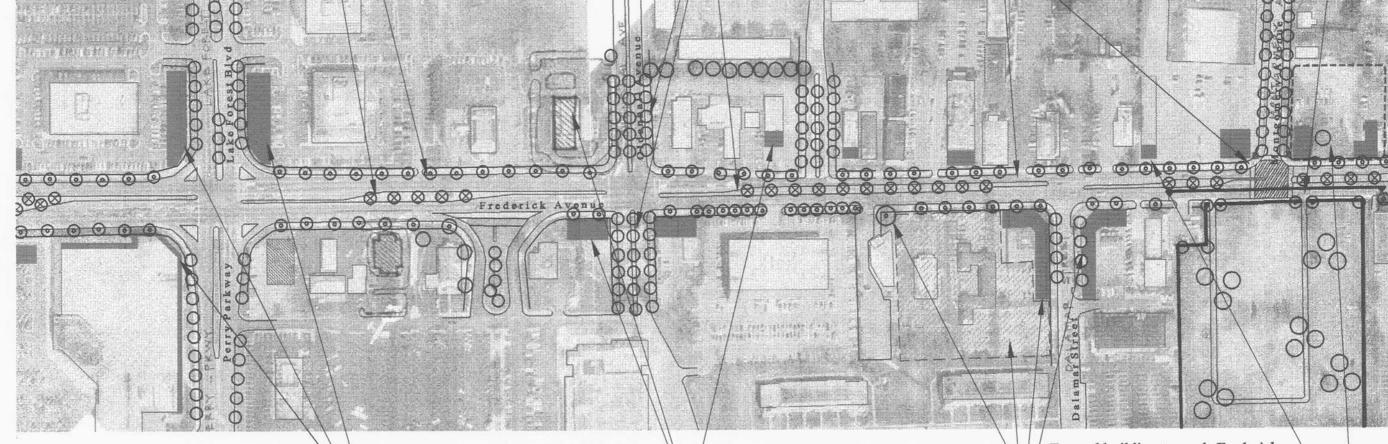
Remove the suicide lane and replace with a median planted with deciduous street trees. See Landscape Architecture Regulations.

Extend Odenhall Road.

Plant side streets with street trees. See Landscape Architecture Regulations. Incorporate neighborhood markers, street trees, street lights, and bulb-outs on residential side streets. See Landscape Architecture Regulations.

Narrow traffic lanes to create a 4'-0" bike lane along the outside lanes of both north and south bound lanes.

Rejuvenate Forest Oak Cemetery with a new gateway fence along the Frederick Avenue frontage, evergreen hedges around the cemetery's perimeter, and infill of mature specimen trees. See Design Code Application for a concept sketch. (This initiative should be a public/private partnership.)



Architectural transformation building. See design code application for concept sketches.

Infill with retail liner buildings. Reconfigure parking to combine lots and increase parking. Infill with retail liner buildings along
Frederick Avenue frontage.

Proposed McDonalds Restaurant

Expand building toward Frederick
Avenue to create a more consistent —

built edge.

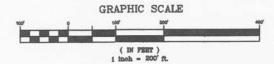
Uplight the existing mature White Oak-

Combine existing parking lots and provide access to side street.

Infill with retail liner buildings along Dalamar Street and Frederick Avenue.

Expand building towards Frederick Avenue to create a consistent built edge.

Develop parcel under one master plan.
Re-zoning the parcel for mixed use and retain the corner building as a commercial space. The former Victor Litz building may be demolished for redevelopment.
Uplight existing specimen tree.



Continue the replacement of suicide lanes with planted medians.

Incorporate neighborhood markers, street trees, street lights, and bulb-outs on residential side streets. See Landscape Architecture Regulations.

Extend McBain Avenue to tie into Perry Parkway.

Establish Chestnut Street Promenade as the primary pedestrian access to the Fairgrounds. Place Fairground Entrance Markers at the intersection with Frederick Avenue. Increase retail frontage along Chestnut Street. See

Design Code Application for concept

sketches of the entrance promenade.

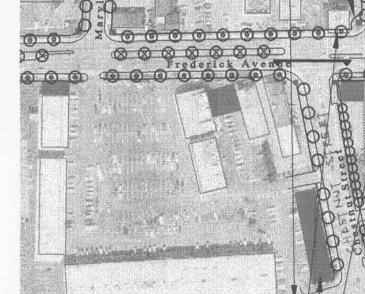
Introduce a new tower as a visual landmark along the Corridor.

Renovate Father Cuddy Bridge as a prominent feature along Frederick Avenue. See the Design Code Application for concept sketches.

Introduce traffic calming measures at the intersection.

Preserve the historic railroad bumper and incorporate into a corner plaza.

Rename the street to recall the demolished historic Trundell House.



Architectural transformation building. See Design Code Application for sketches.

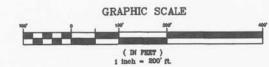
Infill with retail liner buildings. Reconfigure parking to combine lots and increase parking.

Uplight Grace United Methodist Church.

Redevelop parcel to emphasize building frontage along the streets andreconfigure the parking to the rear of the buildings.

No Bain Avenue

Redevelop parcel to emphasize building frontage along the streets.



Establish a strong connection to Olde Towne Gaithersburg. Reconfigure the intersection with a traffic island, new stone walls (recalling those at City Hall), and additional landscaping.

Redevelop site for office use. New

development should conform to Olde Towne architectural guidelines.

Redesign the existing Bohrer Park sign by removing the Gaithersburg High School sign and creating two separate park and high school signs.

Redevelop this parcel into an activity zone in order to encourage community use after school hours. The proposed use is an ice hockey rink and skate board park. See Design Code

Application for concept sketches.

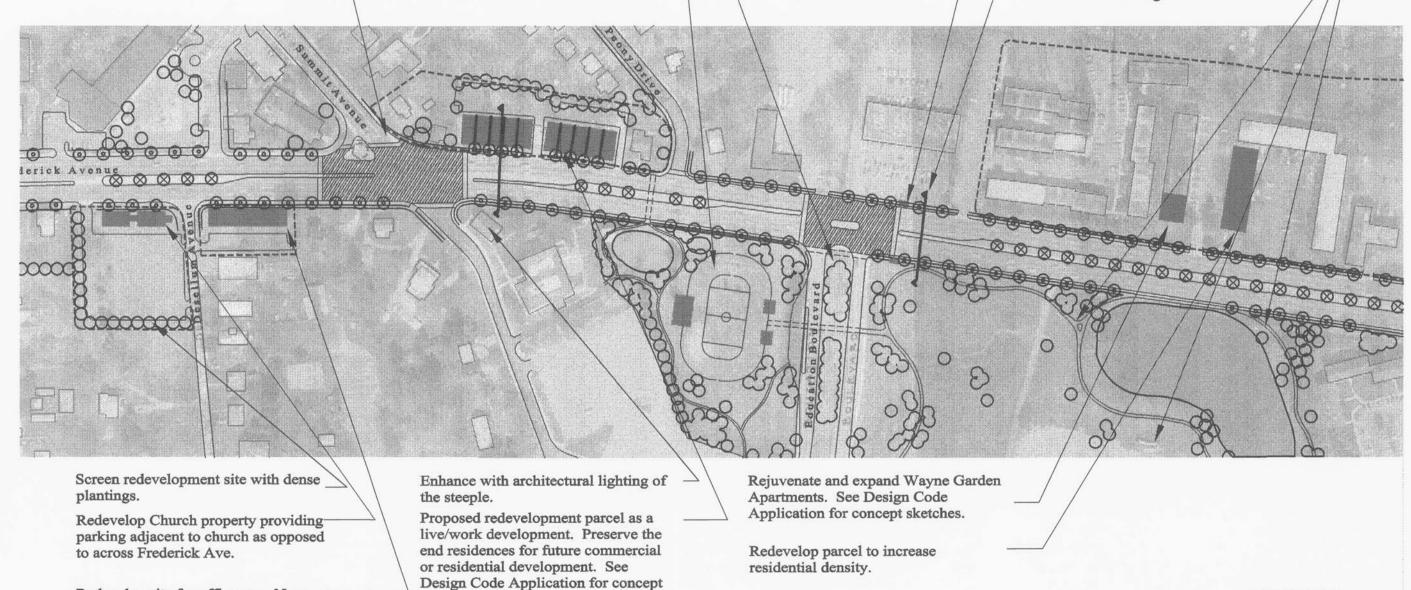
sketch.

Reconfigure Education Boulevard entry to high school and Bohrer Park with a new landscape treatment of median, a new sign and special paving for the intersection with Frederick Avenue. See Design Code Application concept sketch.

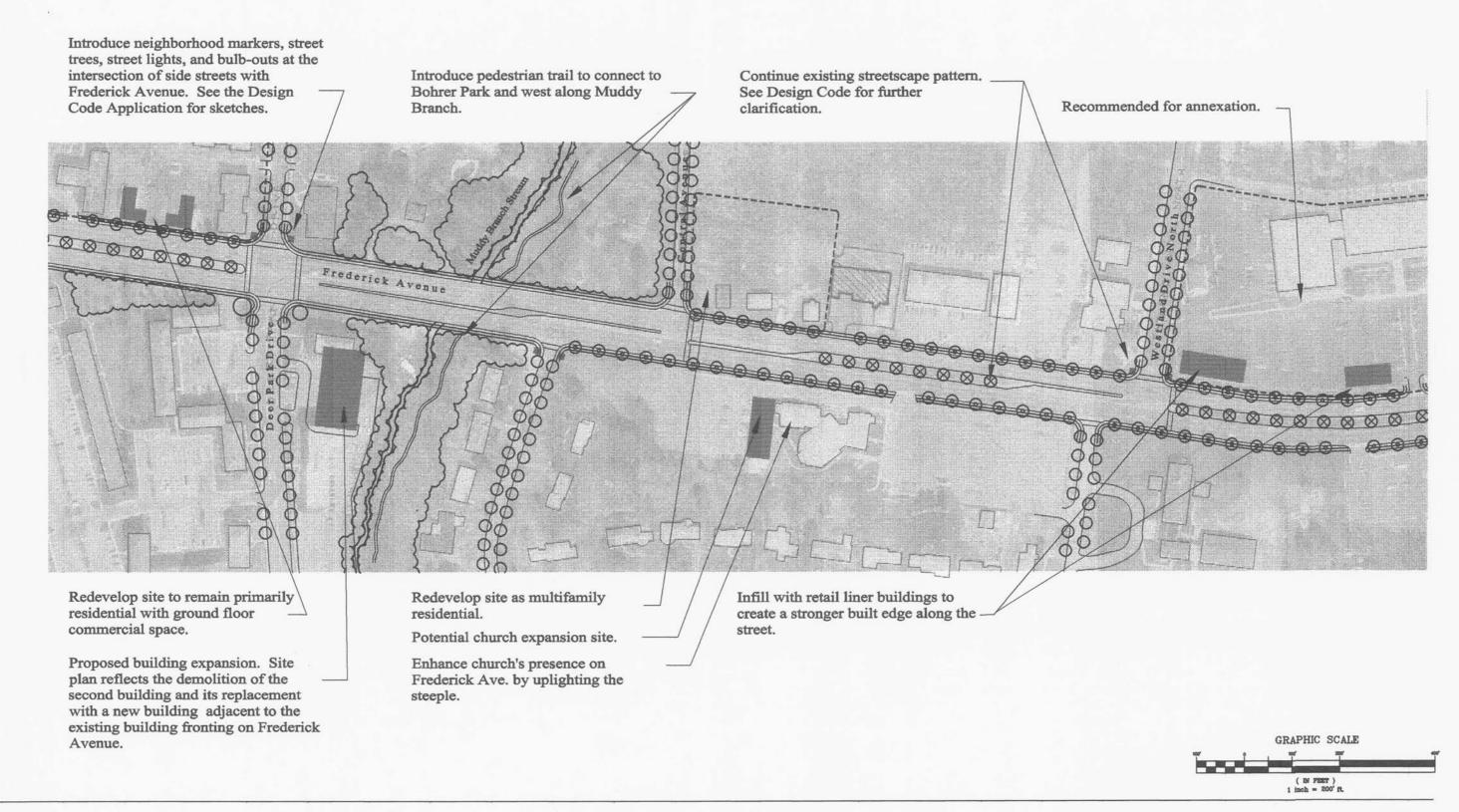
Design two new bus shelters as part of the park redevelopment.

Redesign and extend head-wall to allow for additional signage and lighting, and remove existing panel sign.

Connect the park pathways to the sidewalks along Frederick Avenue.



GRAPHIC SCALE

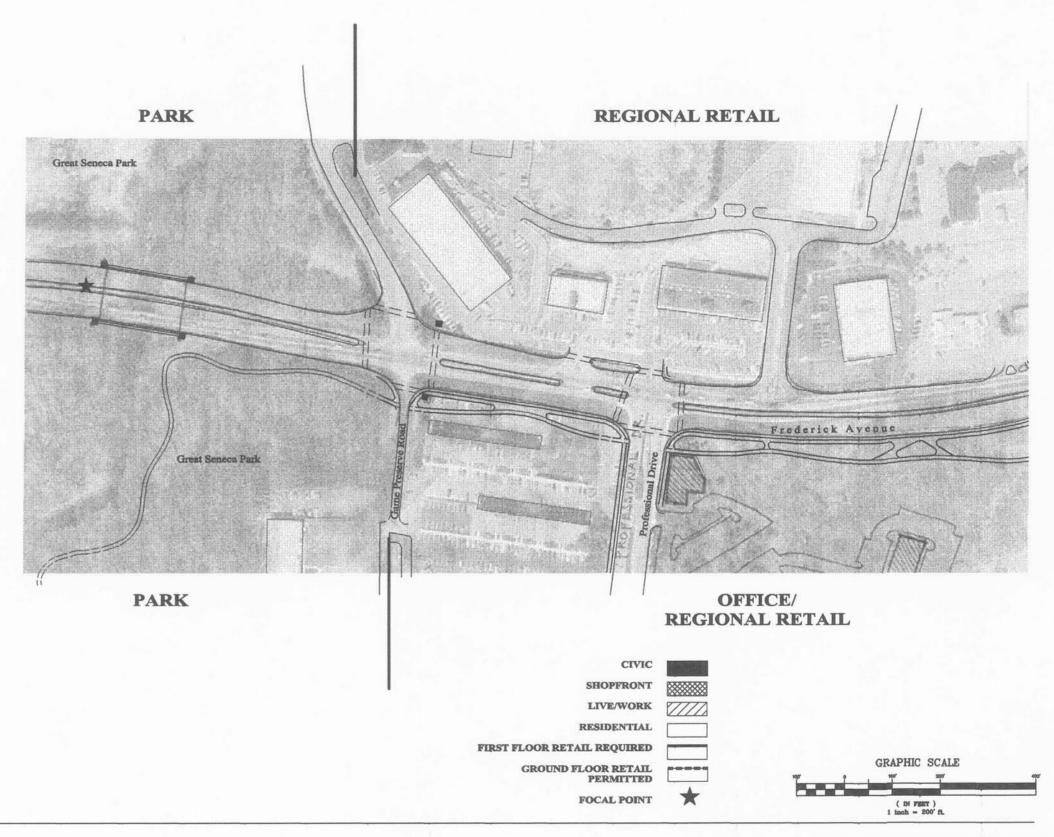


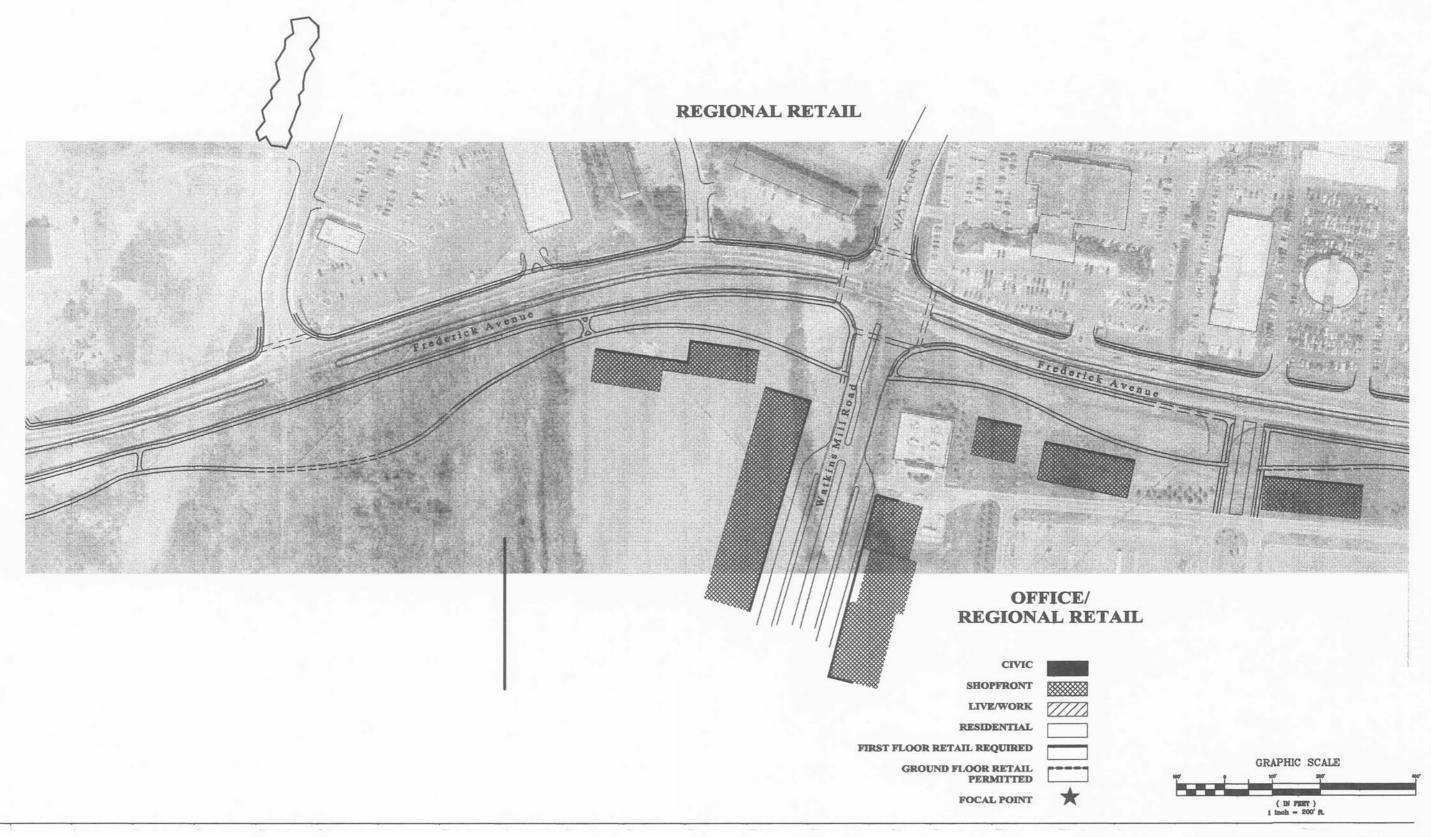
Infill with retail liner buildings along

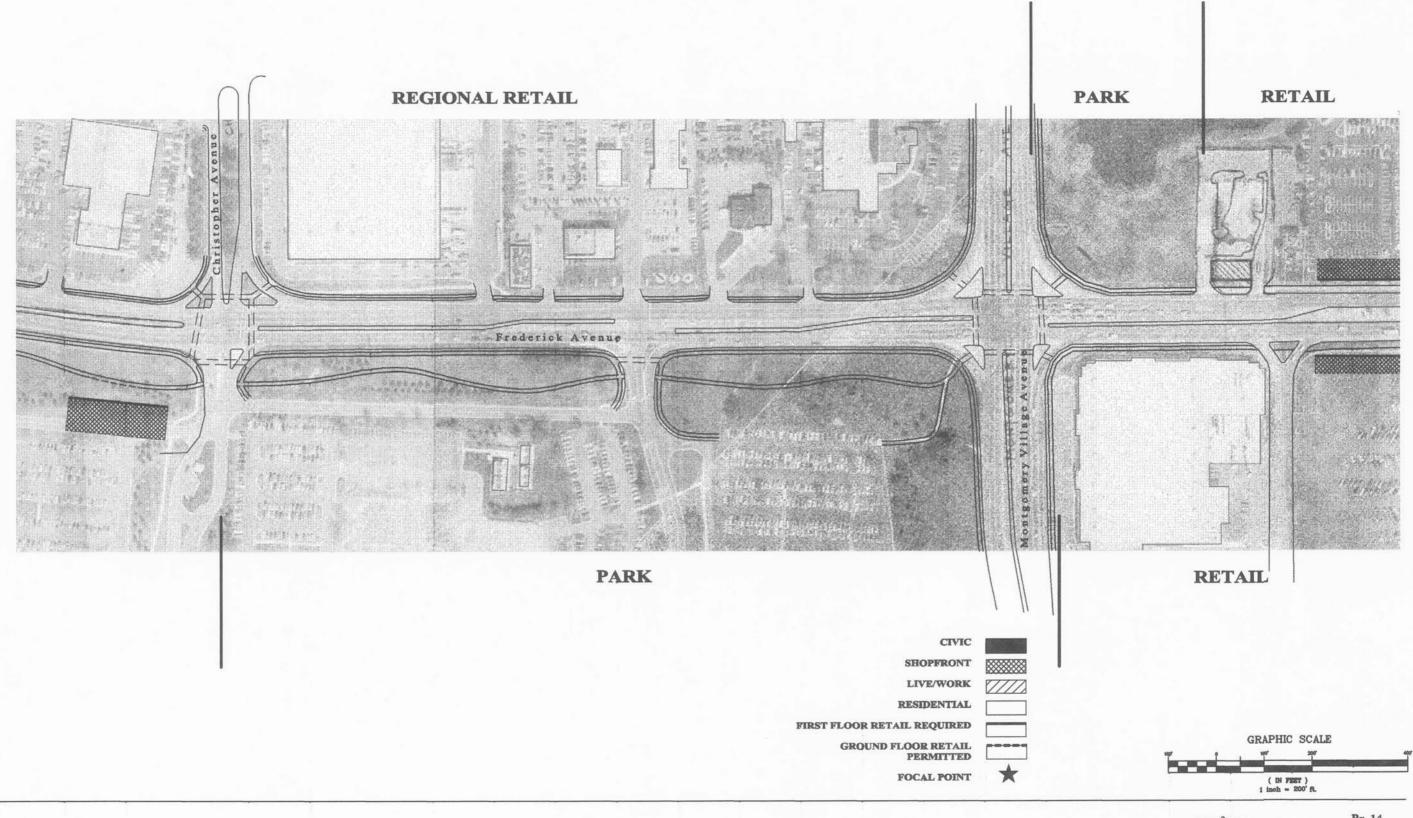
Frederick Avenue.

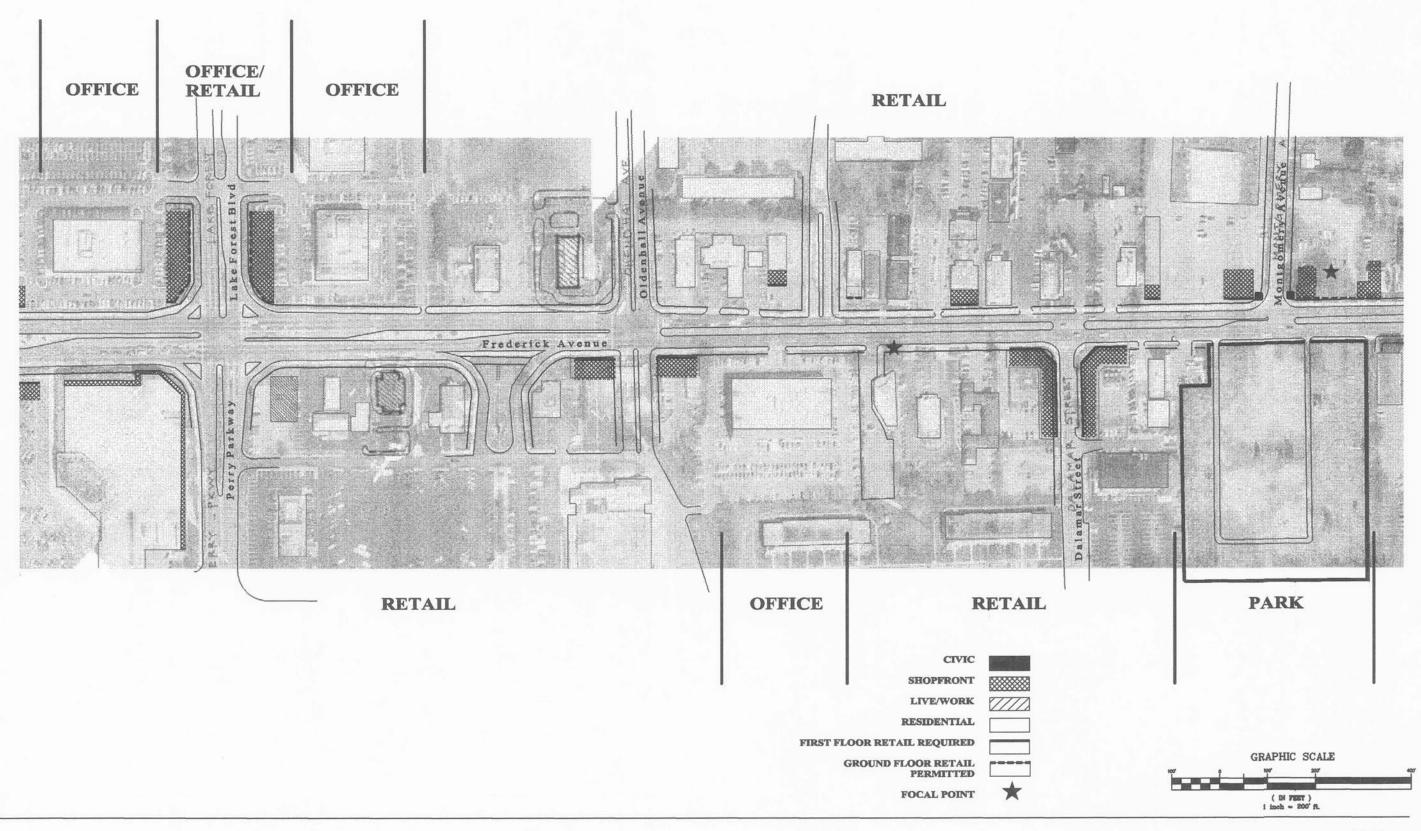
Enhance the I-370 bridge as a gateway into the City. Proposed treatments Recommended site for annexation. include a new "Welcome to Re-zone as office use and match the Gaithersburg" sign, new light fixtures, orientation of the Casey Foundation new cladding, and painting. See the Building across Frederick Avenue. Annex, redevelop and re-zone parcel Design Code Application for concept for office use. Introduce neighborhood markers, street sketches. Remove existing asphalt panel, replant trees, street lights, and bulb-outs at Create the "Oak Grove" gateway. See neighborhood side streets. See with indigenous plant material to match the State Highway planting. Landscape Architecture Regulations Landscape Architecture Regulations. for clarification. 888888888

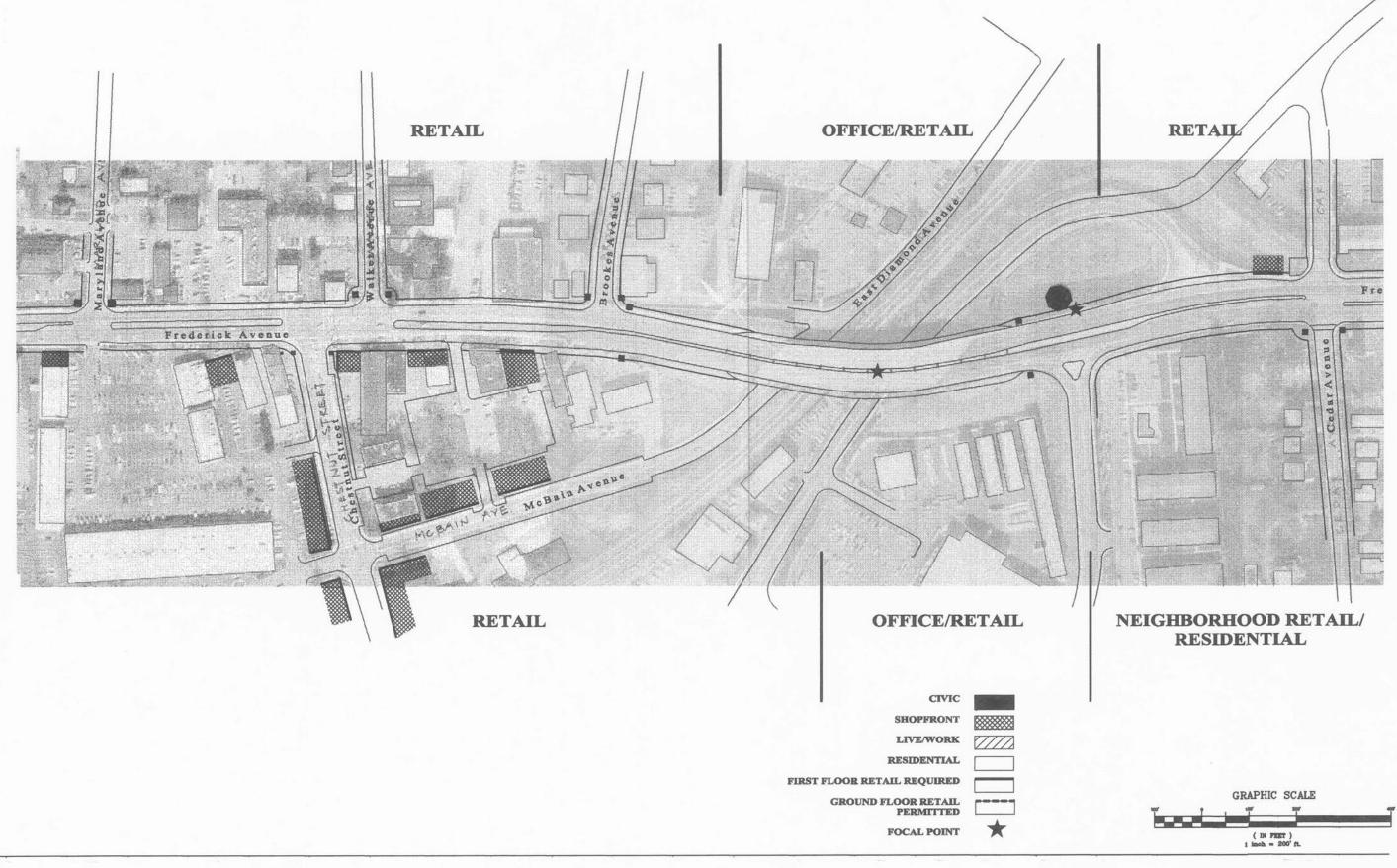


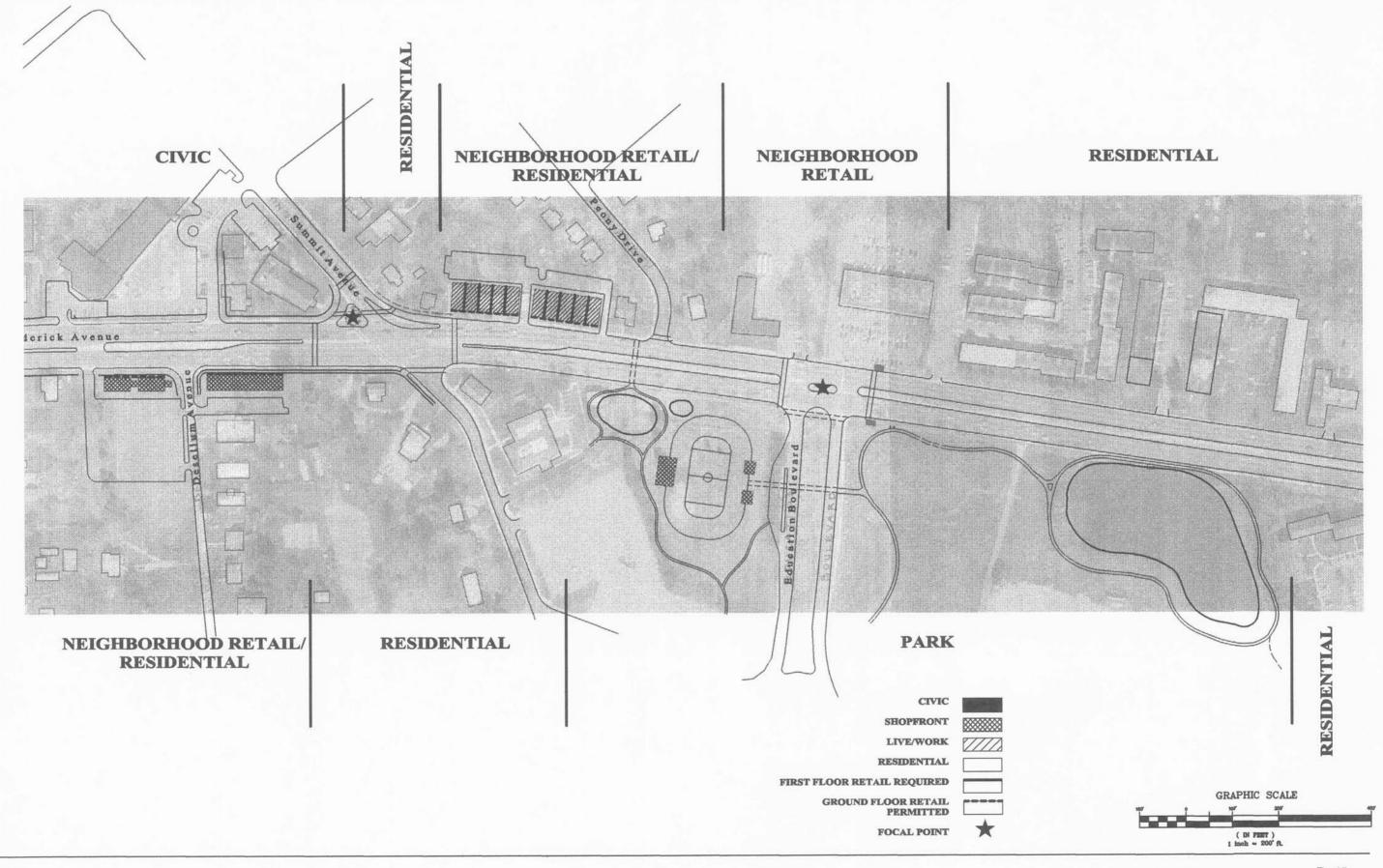


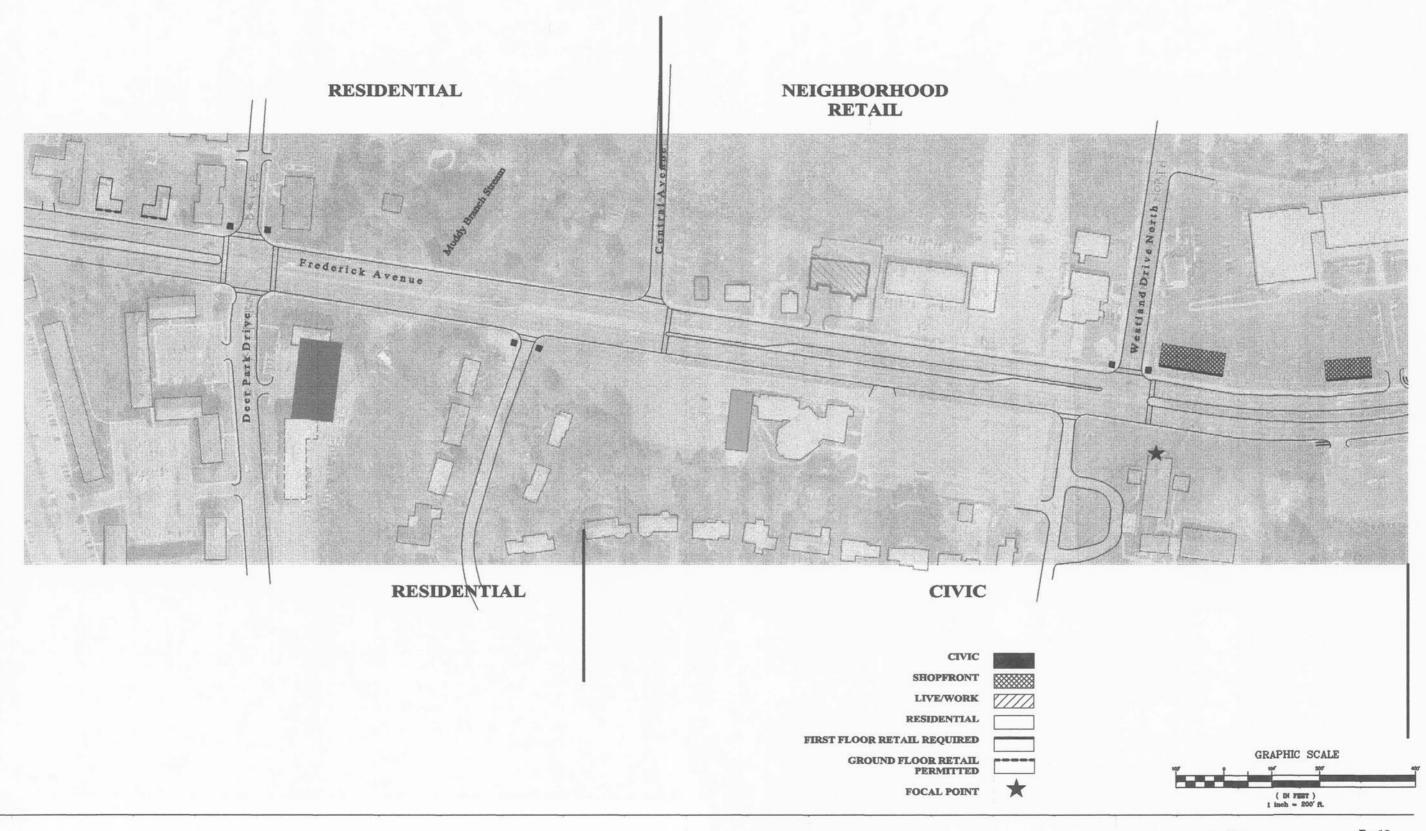


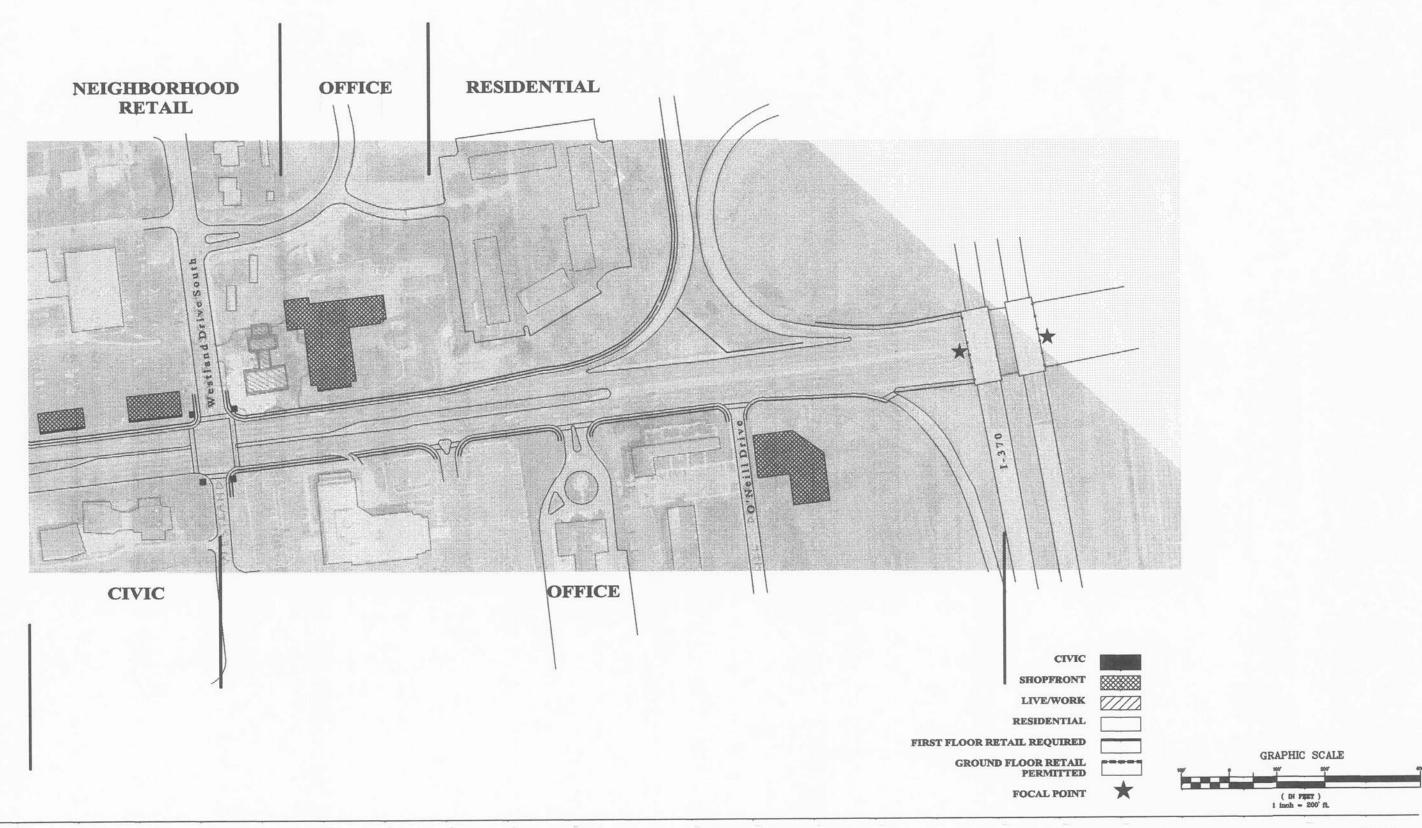












THE DESIGN CODE

PURPOSE

- 1. Encourage a form of development, consistent with the goals of the City, that will achieve the physical characteristics necessary to enhance the economic vitality of the Frederick Avenue Corridor.
- 2. Ensure that new buildings are compatible with their context and the traditional character of the neighborhood, that all uses relate to the pedestrian as well as the automobile, that renovation be equitable for all scales of ownership, and that the permitting process be simplified and facilitated.

GENERAL STANDARDS

- The Design Code shall govern the design of all publicly and privately owned land within the Frederick Avenue Corridor.
- A demolition permit may be obtained only after a site development plan for new construction, which adheres to the Design Code, has been awarded for that property and the site has been reviewed pursuant to the City's demolition plan permit process.

SPECIFIC STANDARDS

Building Height:

Southern Residential District

The maximum building height shall be three (3) stories. Height of new buildings at sidewalk level shall be a minimum of fourteen (14) feet in height from finished floor to finished ceiling (in order to prevent the insertion of mezzanines). A transition line shall be provided between the first and second stories. In retail buildings, the transition shall be detailed to facilitate an awning. Commercial office structures may request an additional height limit not to exceed four stories.

Fairgrounds Commercial District

The maximum building height shall be four (4) stories. Height of new buildings at sidewalk level shall be a minimum of fourteen (14) feet in height from finished floor to finished ceiling (in order to prevent the insertion of mezzanines). A transition line shall be provided between the first and second stories. In retail buildings, the transition shall be detailed to facilitate an awning.

Northern Employment District

The maximum building height shall be six (6) stories. Height of new buildings at sidewalk level shall be a minimum of fourteen (14) feet in height from finished floor to finished ceiling (in order to prevent the insertion of mezzanines). A transition line shall be provided between the first and second stories. In retail buildings, the transition shall be detailed to facilitate an awning.

Building Placement: Buildings and their elements shall be placed on lots as follows:

- a. Front building facades shall be located at the frontage line as shown on the Regulating Plan. Lanes for drive through windows shall not be located between the street and the building front.
- b. In the event of adjacent preexisting setbacks, an adjustment may be allowed or required by the Development Review Team (DRT).
- c. In the absence of a building facade, a screenwall shall be built along the frontage line aligned with the adjacent building facade. Screenwalls shall be between two and one-half (2 ½) and three and one-half (3 ½) feet in height and made of brick, stone or other masonry material matching the building. Screenwalls that are not for the purpose of screening off-street parking lots may be a continuous, maintained evergreen hedge. Screenwalls may have openings up to a maximum width of twenty-five (25) feet to allow vehicular and pedestrian access.
- Loading docks and service areas shall be permitted only within rear yards. Doors for access to interior loading

Urban Regulations

- docks and service areas shall not face a public street.

 e. Off-street parking shall not be permitted within twenty
 (20) feet of any building facade on a frontage line or
 between the building facade and a frontage line.
- f. All buildings shall have their principal pedestrian entrance on a frontage line.
- g. Rear elevations of buildings shall be screened with walls, fences or hedges.

Building Use: Buildings shall accommodate the following range of uses for the various zones designated on the Regulating Plan of the Frederick Avenue Corridor Master Plan:

- a. Uses shall be limited to those allowed in each underlying zoning district.
- b. Single buildings may have multiple uses. Buildings designated "live/work" shall have retail or office space on the ground floor and office and/or residential space on the upper floors. In all cases, retail uses must occupy ground floor spaces when so designated on the Regulating Plan of the Frederick Avenue Corridor Plan. Areas designated "Retail Permitted" may have retail use on the ground floor.
- c. Retail, office or residential uses are required to have a minimum depth of 20 feet from the frontage line on all stories. The remaining depth may be used for off-street parking. Parking access on a frontage line shall be an opening a maximum of 25 feet wide. Openings for parking garage access shall repeat the same rhythm and proportion as the rest of the building to maintain a consistent streetscape.
- d. Seating for outdoor dining shall be allowed to encroach upon the public sidewalks, with the exception of a five (5) foot clear pedestrian passage adjacent the building. This provision shall not exempt the applicant from obtaining the necessary permission for use of public space. Vehicular and pedestrian safety and access for utilities and other public agencies shall not be compromised.

Parking Requirements:

- a. For all nonresidential uses in the Frederick Avenue Corridor, parking may be provided on adjacent streets.
- b. For all residential uses located within the Frederick Avenue Corridor Master Plan, the on-site parking requirements may be met through leasing the required spaces from an off-site parking area.
- c. All off-street parking shall comply with the screening requirement contained in the Landscape Architectural Regulations.

THE DESIGN CODE

Architectural Regulations

WALLS

Materials

- 1. Walls shall be clad in wood, cedar shake siding, hard-board siding, lightweight concrete siding, vinyl siding, stucco, EIFS, brick or stone.
- 2. **Exposed foundation walls** shall be brick, stone, parged block or painted, stained or gray poured concrete.

Methods

- 1. Walls of more than one material shall only change material along a horizontal line (not a vertical or diagonal line) and the heavier material shall always go beneath the lighter material.
- 2. Siding, except vinyl, shall be painted.
- 3. **Brick** shall be in a horizontal running bond pattern.
- 4. Stucco shall be have a smooth finish.
- 5. **Poured concrete** shall have a smooth finish.
- 6. **Trim** is required where there is a change in material or a change in plane.

ELEMENTS

Materials

- 1. Porches shall be wood, or faced with brick or stone.
- 2. Stoops shall be brick, stone or poured concrete.
- 3. **Posts, columns, and railings** shall be wood, steel or wrought iron.
- 4. Piers shall be brick or stone.
- 5. Arches shall be brick, stone or precast concrete
- 5. Chimney enclosures shall be brick, stone or stucco.

Methods

- 1. Wood shall be painted or stained with an opaque or semisolid stain, except walking surfaces which may be unfinished
- 2. Wood posts shall be no less than 6" by 6".
- 3. Piers shall be no less than 12" wide and 8" thick.
- 4. Arches shall be no less than 8" thick.
- 5. Spaces between spindles and balusters shall not exceed 3"
- 6. **Balconies** shall extend beyond the face of the wall no more than 4' and shall be supported by exposed beams or brackets.
- 7. **The undercroft of porches** shall be enclosed with masonry or shall be skirted by wood or vinyl lattice.
- 8. **Decks** may be placed on rear elevations only.

ROOFS

Materials

 Pitched Roofs shall be galvanized metal standing seam, copper, wood shakes, natural slate or asphalt shingles.

Methods

- 1. **Roofs** shall be simple, symmetrically pitched, and in the configuration of gables and hips. The pitch shall be between 6:12 and 14:12.
- 2. Shed roofs shall be permitted when the ridge is attached to an exterior wall. The pitch shall be between 3:12 and 14:12.
- 3. Flat roofs shall edged by a railing or parapet wall.
- 4. Gable ends shall have profile trim.
- 5. **Skylights and solar panels** may be placed on rear elevations only.
- 6. **Roofs** built of steel standing seam shall be painted or galvanized.
- 7. Asphalt shingles shall be black, dark gray or green.
- 8. **Copper** shall be allowed to age naturally.

OPENINGS

Materials

- Windows shall be wood, vinyl clad wood, vinyl or ESP aluminum.
- 2. Glass shall be clear, except stained glass, art glass, and frosted glass of not more than 10 square feet per window. Glass block may be used when not facing a public space.
- 3. **Doors** shall be wood, embossed steel or fiberglass with a wood veneer, and/or glass.
- 4. Doors facing a street shall be hinged.

Methods

- 1. Windows and window lites shall be vertical in proportion.
- 2. Windows shall be rectangular, except that a window may be circular, semicircular, hexagonal, or octagonal in shape, but only one such window may placed on each elevation.
- 3. **Bay windows** facing a street shall extend to the ground or be structurally supported by brackets.
- 4. **Lintels** shall extend beyond the opening a dimension equal to the height of the lintel.
- 5. **Muntins,** if provided, shall be real or mounted on the exterior surface.
- 6. **Two windows** in the same rough opening shall be separated by a minimum 4" post.
- 7. Windows shall be no closer than 2' to the corners of the building.
- 8. Shutters shall be shaped, sized and proportioned to the window(s) they serve.
- 9. **Shutters** shall be functional or provided with shutter dogs and mounted as if hinged to the window frame.
- 10. **Doors**, including garage doors, shall have glass, raised panels or both.
- 11. Storm doors and screen doors shall be full view and free of decorative trim.
- 13. Commercial garage doors shall not face a public street.

Architectural Regulations

GARDENS

Materials

- 1. **Fences & gates** shall be wood pickets, wood lattice in combination with wood boards or pickets, wood boards, steel, wrought iron or ESP aluminum.
- 2. Garden walls shall be brick or stone.
- Retaining walls shall be brick or stone except those in rear yards which may be poured concrete or pressure treated wood.
- 4. Walks and paths shall be brick, stone, slate, concrete (gray or stained) or concrete payers.

Methods

- 1. **Terminal posts in fences** (corners, openings, ends, etc.) shall be thicker and taller than other posts.
- Fences, walls and hedges (not specifically intended as screening devices) along streets, sidewalk, parks, etc., shall be between waist and chest height (the applicant's height).
- 3. Wood fences shall be painted white, or stained white with an opaque stain.
- 4. Steel or wrought iron fences shall be painted black or dark green.
- 5. **Brick walls** shall be a minimum of 8" thick and capped. The cap shall overhang a minimum of 1/2".
- 6. **Retaining walls** shall be between knee and chest height (the applicant's height), measured from finished grade.
- 7. **Poured concrete** shall have a smooth finish.
- 8. **Patios** may be placed only on rear elevations.

STOREFRONTS

Materials

- 1. **Storefronts** may be, in addition to door materials previously stated, aluminum.
- 3. **Awnings** shall be built of a metal structure covered with canvas or solution-dyed acrylic fabric ("Dickson-Elberton", "Sunbrella" or equal).

Methods

- 1. Storefronts shall be designed individually.
- 2. The storefront, doors, awnings and signage shall be a unified design.
- 3. The storefront should be detailed while the rest of the building should be simple.
- 4. **Doors** shall be flush with the building wall or recessed a minimum of 3'. Doors shall be located on the corner on corner lots.
- 5. **Storefront** glazing shall be a minimum of 75% of the first floor elevation.
- 6. Glass shall be clear.
- 7. **Awnings** shall be permitted to encroach the full width of the sidewalk, but may not extend within 1' of the curb.
- 8. Awnings shall be rectangular in shape with straight edges.

 Awnings may have side panels but shall not have a "bottom" panel (a panel parallel to the ground).
- 9. Awnings shall not be backlit.
- 10. Awnings shall be within reach at the drip edge.
- 11. The vertical drip of an awning may be stenciled with signage a minimum of 8" in height.
- 12. **Street elevations of corner buildings** shall be illuminated by fixtures attached to the building.

1. **Variances** may be granted on the basis of architectural merit by the Development Review Team (DRT).

MISCELLANEOUS

- The following shall be located in rear yards only: HVAC equipment, meters, solar panels, antennas, satellite dishes, trash enclosures, dumpsters and garbage cans. Mechanical equipment may be placed on the roof if properly screened from public view, as determined by the DRT.
- 3. The following shall not be permitted: panelized materials, copper anodized aluminum.
- 4. Exterior light fixtures shall be compatible with the style of the building they serve.

Landscape Architectural Regulations

Southern Residential District

Softscape Elements

Oak Groves at the I-370 gateway shall be a mix of a minimum of five species of oaks and range in size from whips to 4 1/2" caliper trees. The spacing and placement shall be irregular and informal, and the number of trees shall be a minimum of 6.5 trees per 100 square feet.

Frederick Avenue Street Trees shall reinforce the existing pattern of trees. Infill and replace decaying or missing trees with Quercus coccinea (Scarlet Oak) for both the road edge and median plantings.

Secondary Edge Trees shall be planted by property owners with over 260 linear feet of frontage. The secondary edge trees shall be planted within a maximum distance of 30 feet from back of curb. The number of trees shall be 1 per 120' of frontage.

Side Streets shall be planted with Quercus imbicaria (Shingle Oak) at a spacing of 38'-0" on center for the first 600 linear feet of roadway starting at Frederick Avenue.

Hedges shall be required where parking lots are located within 30' of back of curb. Hedges shall be evergreen with growth habits that may be pruned to a minimum of 2 1/2 feet and a maximum height of 4 feet.

Medians shall have planting beds that are a minimum of 26' in depth and planted with evergreen groundcovers that have a maximum height of 24 inches at the end points. The remaining ground plane planting shall be lawn.

The Lawn Verge (existing on the sides of Frederick Avenue) shall be 6'-0" in width and be continuous along Frederick Avenue.

Hardscape Elements

The Bridge at I-370 shall be painted, reclad with precast panels, new lights added at 30'-0" on center on both sides, and a new sign panel affixed to the bridge. See concept sketches

The fencing around the I-370 bridge abutments shall be replaced with decorative metal fencing a minimum of 5 feet in height and painted black or dark green..

Sidewalks shall be a minimum of 6 feet wide. In areas of redevelopment, it is strongly recommended to move the sidewalk to the outside of the existing street trees, so that the trees separate the sidewalk from the street.

Crosswalks shall be painted with white stripes.

Neighborhood Side Streets shall be announced with markers at the entrances to the streets of Frederick Avenue.

The markers shall be of an architectural style that relates to the architecture of the community. See the concept sketches in the Design Code Application..

Furnishings

Benches shall be wood with arm rails.

Bike racks should be placed near bus shelters. The bike racks shall be painted metal to match the benches.

Trash receptacles shall be painted metal, 42-gallon in capacity, with a cover over the opening. The receptacles shall be painted the same color as the bike racks and benches. Recycling receptacles should accompany the standard receptacle and be labeled accordingly.

Bus Shelters should be the adopted "Gaithersburg Shelter".

Fairgrounds Commercial District Softscape Elements

Oak Groves at the Father Cuddy Memorial Bridge Gateway shall be a mix of a minimum of four species of oaks and range in size from whips to 4 1/2" caliper trees. The spacing and placement shall be irregular and informal, and the number of trees shall be a minimum of 5.5 trees per 100 square feet.

Frederick Avenue Street Trees for both medians and sidewalks shall be Quercus robur 'Forest Green' (Forest Green English Oak). The sidewalk trees shall be planted in 6'-0"x6'-0" tree pits with metal tree grates adjacent to the street. The median trees should be planted within the planting bed area and underplanted with Liriope muscari. Side Streets in this area shall be planted with Quercus macrocarpa (Bur Oak) at a spacing of 38'-0" on center for the first 600 linear feet of roadway starting at Frederick Avenue.

Hedges shall be required where parking lots are located within 30' of back of curb. Hedges shall be evergreen with growth habits that may be pruned a minimum of 2'-6" and maximum of 4'-0" in height.

Medians shall have a planting bed that is a minimum of 26' in depth from the end points, planted with both annuals and perennials of species that have a maximum height of 24 inches. The remaining ground plane shall be planted with Liriope muscari and have a brick edge treatment.

Hardscape Elements

The Father Cuddy Memorial Bridge shall receive a similar treatment as the I-370 Bridge. This treatment includes new precast concrete piers, additional lights, and new cladding. See Design Code Application for concept sketches.

The Fencing on the Father Cuddy Memorial bridge abutments shall be replaced with decorative metal fencing meeting safety standards and painted a dark green or black.

Sidewalks shall be a minimum of 6 feet wide and a maximum of 25' wide to extend between the face of building and the street.

Crosswalks shall be painted with white stripes.

Furnishings

Benches shall be painted metal with arm rails.

Bike Racks should be spaced every 500 linear feet of roadway or grouped in bike lots near the bus shelters.

The bike racks shall be painted metal to match the benches.

Trash receptacles shall be painted metal, 42 gallon in capacity, with a cover over the opening. The receptacles shall be painted the same color as the bike racks and benches. Recycling receptacles should accompany the standard receptacle and be labeled accordingly.

Bus Shelters should be the adopted "Gaithersburg Shelter"

Northern Employment District

Softscape Elements

Oak Groves at Game Preserve Road shall be a mix of a minimum of five species of oaks and range in size from whips to 4 1/2" caliper trees. The spacing shall be irregular and informal, and the number of trees shall be a minimum of 6.5 trees per 100 square feet.

The Oak Grove at the intersection of Montgomery Village Avenue and Frederick Avenue shall be planted with a double row of Quercus accutisma (Sawtooth Oak). The spacing shall be 46'-0" on center along the row, and each row shall be offset by 24'-0".

Frederick Avenue Street Trees shall reinforce the existing pattern of trees established by the State Highway Administration up to Montgomery Village Road. Infilling and replacing of decaying or missing trees shall be done with Quercus phellos (Willow Oak) for both the road edge and median plantings.

Secondary Edge Trees shall be required to be planted by property owners with over 240 linear feet of frontage. The secondary edge trees shall be planted within a maximum distance of 30 feet from back of curb. The number of trees shall be 1 per 90 linear feet of frontage.

Side Streets shall be planted with Quercus imbicaria (Shingle Oak) at a spacing of 38'-0" on center for the first

600 linear feet of roadway starting at Frederick Avenue. **Hedges shall** be required where parking lots reside within 30' of back of curb. Hedges shall be evergreen with growth habits that may be pruned to a minimum of 2 1/2 feet and a maximum height of 3 feet.

Hedge Rows consisting of a mixture deciduous trees and evergreen trees, shall be planted along the property lines of all property greater than 3 acres. The tree spacing shall be a minimum of 10 feet on center.

Medians shall have a planting beds that are a minimum of 26' in depth from the end points and planted with evergreen groundcovers that have a maximum growth height of 24 inches. The remaining ground plane planting shall be lawn.

Hardscape Elements

The Bridge at Great Seneca Creek should receive upgrades equal to the I-370 Bridge. These upgrades shall include lighting, new piers, recladding of precast concrete panels, and landscaping. See the Design Code Application for concept sketches..

Sidewalks shall be a minimum of 6 feet wide. In areas of redevelopment, it is strongly recommended to move the sidewalk to the outside of the existing street trees to create a continuous 6'-0" minimum lawn panel adjacent to the street.

Crosswalks shall be painted with white stripes.

Furnishings

Benches shall be wood with arm rails.

Bike racks should be spaced every 2,500 linear feet of roadway or closer. The bike racks shall be painted metal the same color as the benches.

Trash receptacles shall be painted metal, 42-gallon in capacity, with a cover over the opening. The receptacles shall be painted the same color as the bike racks and benches. Recycling receptacles should accompany the standard receptacle and be labeled accordingly.

Bus Shelters should be the adopted "Gaithersburg Shelter".

Landscape Architectural Regulations

Special Areas

Bohrer Park shall be highlighted by additional mature specimen tree plantings within the park. A new sign/wall shall be designed along the headwall of the pond to increase the park's visibility and identity toward the south. The billboard sign within the park shall be removed.

Two new bus shelters shall be designed and placed in the Park and across Frederick Avenue.

Special paving materials shall be used at the intersection of Education Drive and Frederick Avenue.

A new sign panel (as shown in the Design Code Application) shall replace the existing sign panel and the high school shall receive a separate monument sign.

Forest Oak Cemetery shall receive an new iron gate and iron fence treatment as diagramed in the Design Code Application. Additional deciduous trees shall be planted within the cemetery and evergreen hedge planted along the property lines.

The Chestnut Street Promenade is intended to highlight the major pedestrian access to the Fairgrounds. It is also intended to create some new retail/commercial spaces that will draw people to the Fairground area. The envisioned treatment is to include the application of a special paving material to Chestnut Street, a new Fairgrounds entry marker that will accept promotional banners to advertise events, new pedestrian lighting, and narrowing the street width to increase more sidewalk area.

The **Frederick Avenue Linear Park** along the west side of North Frederick Avenue is intended to be a passive open space varying in depth and planted with a mixture of plant species. A secondary path 12'-0" wide shall meander through the "woods".

Lighting Regulations

Street Lighting for the entire corridor shall be done using the existing utility poles and replacing the existing cobra head fixtures with a new decorative bracket and "tear drop" shaped glass luminare. The new lamp source shall be 250-watt metal halide and will provide a wide distribution onto the street. The color of the fixture should be a dark green to black.

Fairgrounds Commercial District street lighting shall be enhanced by installing new pedestrian poles and fixtures. These poles shall be installed every 54'-0" on center and 2'-6" from back of curb. Pole fixture height shall be 14'-0" and shall have a "glow" type luminare with 360 degree illumination pattern. The light source shall be 150-watt metal halide. Pole and fixture color should be a dark green to black. See Design Code Application for examples of this fixture.

Bridges shall have new pole fixtures installed on the outside edge of the bridge. Pole height shall be 18'-0" and shall have a tear drop shaped glass luminaire. The lamp source shall be 150 Metal Halide.

Parking Areas. Lighting when visible from Frederick Avenue shall be limited to a shoe box type luminaire, with 100% downward distribution and without diffusing lenses. Luminaire height shall be a maximum 24'-0" above grade. The light source shall be 150 to 250-watt metal halide. Finish of the fixture to be dark bronze or black. Luminaire shall be parallel with the ground plane, tilting of luminaire and "wall pack" fixtures are unacceptable. An example of a shoe box type fixture is shown in the Design Code Application section of this study. Light pole footings may not extend more than 6" above finished grade unless located in a parking area in which the light pole is not protected by curbing or other measures.

Canopy Structure Lighting. Fixtures shall be 100% downlight distribution, mounted directly over task. Fixtures over gas pumps have a flush lense. Overhang of canopy shall be designed to shield the brightness from the lensed fixtures. Maximum light level to be 20 footcandles on ground plane.

Special Buildings requiring "washed" surface light to be handled by fixtures mounted on the ground, lighting upwards with shields if necessary to prevent spill light onto adjacent properties or glare onto roadways. Light source to be metal halide, (3000K,85CRI) of halogen.

Trees denoted on the plan to receive uplighting shall be done with shielded ground mounted or in ground lights fixtures. Light source shall be 3000K metal halide or halogen.

Buildings within 60'-0" of Frederick Avenue shall have ground mounted wash lighting with shields if necessary to prevent spill light onto adjacent properties or glare onto roadways. Average foot candles on building surface (vertical) shall range from 5-10. Light source to be color corrected metal halide or halogen. Lighting attached to the building shall be of a decorative type only, compatible with the architectural style of the building. Light sources to be incandescent, halogen, compact fluorescent (18 watt max., 2700-300K), metal halide (50 watt max., 85 CRI,

Environmental Graphics Regulations

Monument Signs

One monument sign shall be allowed at each street that the property adjoins to a maximum of two total signs. If two monument signs are approved the sign locations and orientation must not allow both signs to be read in the same view shed.

Street address must be displayed prominently with a minimum number height of nine inches. The font style shall be selected from a palette of fonts supplied by the city.

The overall dimensions of the monument sign shall not exceed 5'-0" in height and 10'-0" in width.

Internally lit signs and neon will not be permitted on monument signs.

Individual letter height shall not exceed twelve inches. Monument signage submittal shall include base landscape and lighting design.

Building Signs

Existing signs consisting of individual channel letters including internally lit, reverse channel and open channel may remain in use provided it meets all other criteria outlined in the new Environmental Graphics Regulations.

Internally lit box signs will not be permitted.

Buildings placed more than 20'-0" from the street curb may use individual channel letters as the main business signs. Multitenant buildings that have tenants both inside the 20' zone and beyond the 20' zone should develop a consistent sign system for all tenants. Individual tenants may not apply for separate sign styles based on distance to individual leased space. Buildings located within 20'-0" of the curb may use non-illuminated letters, blade signs, painted signs or banners. These sign types may all be illuminated using appropriate surface mounted fixtures to light the face of the sign type selected.

The sign size shall be determined by the square footage of the wall it is mounted to. Building signs shall not exceed 10% of the total square footage of the face of building holding the sign and shall have a maximum size of 100 square feet. No roof area between eve and ridge or area from adjacent sides of building shall be included in square foot calculations. When determining sign size for a multi-tenant development, square footage calculations will be limited to the width of the individual leased area for each tenant, not the total square footage of the wall it is attached to if that wall is shared with other tenants.

Each building with a single tenant shall be allowed one sign on

one building face.

Each building with multiple tenants shall be allowed one sign per tenant attached to face of building. The sign size shall be determined by the square footage of the wall it is mounted to within the limits of the individual leased area.

Letter height for multi-tenant developments shall not exceed 3'-0." If an anchor tenant condition exists, those tenants may increase allowable square footage to maximum 200 square feet with a 4'-0" maximum letter height.

Letter height for single tenant properties shall not exceed 3'-0" if the building is located less than 50'-0" from the street curb. If the building is located more than 50'-0" from the curb the total height may increase to 4'-0."

If the property has two main exposures, two signs may be used provided the following criteria is met:

The total square footage of all combined signs on the building does not exceed the limits outlined above The two signs are not visible from the same view shed Signs are mounted parallel to the face of the building they are mounted to

A portion of the signage may exceed height limitations by 12" to accommodate non-text logos, symbols and icons as part of the signage artwork.

All letters in one name shall be the same color. The number of colors in one multi-tenant development shall not exceed two. Neon signs are permitted as a building sign only when constructed as an open channel letter as a main tenant sign. Blade signs shall be constructed of a rigid material. Painted signs of a historical billboard nature shall follow the overall size limitations outlined above.

Window Graphics

One neon sign may be located within the store for external viewing. This sign may contain the tenant's name. Neon signs will be limited to a total area of 5 square feet. Neon signs will be included in the total allowable square footage calculations. Any interior neon signs visible from the store exterior are subject to approval by the City of Gaithersburg.. Hours of business operations and telephone numbers may be placed on entry and exit doors to a total of two square feet. Promotional graphics including but not limited to posters, vinyl adhered to the glass, banners, neon visible from outside and painted glass are all subject to window graphics limitations outlined herein:

The total graphics area combined shall not exceed 20% of the total area of glass with a maximum square footage allotment of 25 square feet per business location.

The Design Review Policy

DESIGNREVIEWPOLICY

- A. Function of the City Development Review Team (CDRT). To encourage architectural harmony and to ensure that all property owners adhere to the Design Code. No structure or improvement shall be erected or altered until the approvals described in this document have been obtained.
- B. Scope of Responsibility. The City will also review all property owners' improvements, including alterations and modifications to structures (even after initial construction is complete), that are visible from the street and neighboring residences, including, but not limited to walls, painting, renovations, and landscaping.
- C. Enforcing Powers. Should a violation occur, the City has the right to an injunctive relief which requires the owner to stop, remove, and/or alter any improvements in a manner that complies with the standards established by the City of Gaithersburg Planning and Code Administration..
- D. Limitations of Responsibilities. The primary goal of the CDRT is to review applications and determine if the proposed improvement(s) conform to the Design Code and make recommendations to the City Planning Committee. The CDRT does not assume responsibility for the following:
 - Structural adequacy, capacity, or safety features.
 - Non-compatible or unstable soil conditions, soil ero sion, etc.
 - Compliance with any and all building codes, safety requirements, and governmental laws, regulations or
 - The performance or quality of work of any contractor.
- E Committee Members. The CDRT shall consist of individuals representing the Department of Planning and Code Administration, the Department of Public Works, the Police Department, and the Department of Recreation. The CDRT may consist of other representatives appointed by the City, including a Town Planner, an Architect and/or a Landscape Architect.
- F. Administration. The Planning and Code Administration Staff of the City of Gaithersburg will process applications. Review fees will be established by the Mayor and City Council.
- **G** Review Policies. The Planning and Code Administration Staff of the City of Gaithersburg establishes all procedures for the processing of applications. Guidelines are available through the Department of Planning and Code Administration of the City of Gaithersburg, located at 31 South Summit Avenue, Gaithersburg, Maryland. (301) 258-6330.

- H. Exceptions to the Design Code. All requests for exceptions to the Design Code must be made in writing to the CDRT. Any exceptions granted shall be considered unique and will not set precedence for future exceptions.
- I Construction Bond or Letter of Credit. A construction deposit of 1% of the construction cost is required from the applicant for proposed improvements. The deposit check shall be made payable to City of Gaithersburg. The deposit shall be held by the Department of Planning and Code Administration (P&CA) until the building passes the Field Inspection and a Letter of Compliance has been issued by the P&CA. Full compliance will result in the return of the deposit. If the deposit is required to repair, replace, or clean up common areas that are damaged due to construction activities or to bring the building into compliance with the approved application, the applicant will be notified forty-eight hours prior to the use of the deposit. This allows the builder a final opportunity to rectify the problem before the deposit is expended.
- J. Construction Observation. Periodic observations may be made by the P&CA to verify compliance of the improvements with the approved applications.
- K. Waiver and Additional Requirements During Construction. The Design Code has been adopted to assist property owners and the CDRT with the Design Review Procedure. However, these are guidelines, and when deemed appropriate by the CDRT, the CDRT has the right to waive some or all of the requirements for any proposed improvement(s).
- M The Development Review Team (CDRT) receives submittals and other communications.

Address: 31 South Summit Avenue Gaithersburg, Maryland 20877

(301)258-6330 Phone:

(301)258-6336 Fax:

E-mail: planning@ci.gaithersburg.md.usa

DESIGNREVIEWPROCEDURE

Sketches may be submitted to the CDRT at any time for informal, nonbinding comments.

Step I: Schematic Design Review. The intention of this review is to confirm that the applicant has a correct understanding of the Frederick Avenue Corridor Design Code early in the design process. More than one scheme may be submitted for review should the applicant wish to consider the opinions of the CDRT before selecting one scheme to develop further. Submit the following:

- A. Form A: Concept Plan Review Application.
- B. Concept Plan Documents (two sets) including:
 - 1. Location Plan at 1"= 100'.
 - 2. Site Plan at 1''=20'.
 - 3. Site Sections at 1"= 20'.
 - 4. Landscape Plan at 1"= 20'.
 - 5. Signage & Lighting Plan at 1"= 20'.
 - 6. Floor Plans at 1/4"= 1'-0".
 - 7. Elevations at 1/4"= 1'-0".
 - 8. Material Specifications.

Step II. Final Plan Review. This review confirms compliance of the design with the Design Code and verifies that previous recommendations made by the CDRT have been incorporated. Submit the following:

- A. Form B: Final Plan Review Application.
- B. Final Plan Documents (two sets) including:
 - 1. Location Plan at 1''=100'.
 - 2. Site Plan at 1''=20'.
 - 3. Landscape Plan at 1"= 20'.
 - 4. Tree Protection Plan at 1"= 20'.
 - 5. Planting & Maintenance Specifications.
 - Signage & Lighting Plan at 1"= 20' and details.
 - 7. Site Cross Sections at 1"= 20'.
 - 8. Floor Plans at 1/4"= 1'-0".
 - 9. Elevations at 1/4"= 1'-0".
 - 10. Building Sections, two, at 1/4"= 1'-0".
 - 11. Details at 1-1/2"= 1'-0".
 - 12. Material Samples for walls, roofs and windows.

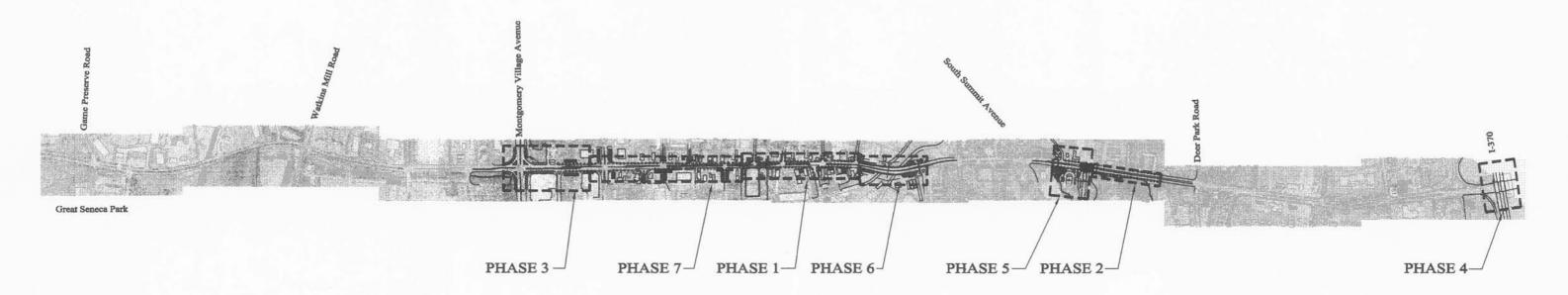
Step III. Minor Change(s). For approval to make minor changes during construction, submit the following:

- A. Form D: Minor Amendment to Final Plan Application
- B. Additional Information to describe the change(s).

Step IV. Field Inspection. This inspection confirms that all built improvements are in compliance with the Design Code and acceptable to the CDRT. Submit the following:

- A. Form E: Post-Construction Field Inspection Applica
- B. Form F: Post-Construction Field Inspection Report.

Phasing Plan



General Notes

The phasing plan represents public initiative projects and their relationship to the development of the corridor. The sequence is structured to spread improvements throughout the length of the corridor.

Phase 1: The first project is to implement a new streetscape treatment for three blocks of Frederick Avenue from Montgomery Avenue to Brookes Avenue. The streetscape treatment should re-stripe the roadway, delete the "suicide lane" and add a median with street trees. Painted bike lanes alone the curb should be provided when restriping the travel lanes. Special treatments should be applied to the cemetery. Please see the "Implementation Plans" following the Phasing Plan for an approach to this area.

Phase 2: The second project consists of landscape treatments for Bohrer Park along the Frederick Avenue frontage. Treatments would include building a new headwall/sign at the southern end of the pond, removing the billboard sign within the park, adding a specially designed bus shelter with stairs and ramps to access the existing walks, and adding metal railing to tie the headwall and bus shelter together.

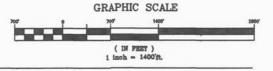
Phase 3: This project is actually a two-phased project. The long-term vision is a grade separated interchange with a roundabout. This project is scheduled to happen in ten years. The interim solution is the establishment of an oak grove with planted medians and timed crosswalks.

Phase 4: The fourth project is the first "gateway" treatment project. The I-370 bridge would receive architectural treatments including new light poles, new gateway signage, new paint, and architectural lighting. This project would also consist of adding the oak grove, and replacing the chain link fence.

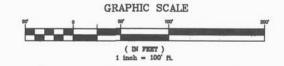
Phase 5: Phase five is a joint venture between the City and the school board to add an ice/roller rink and a skate park to the undeveloped portion of land north of Bohrer Park. This project would be a mechanism for increasing activity along Frederick Avenue. This project would also fulfill an identified community recreation need.

Phase 6: As the sixth project, Father Cuddy Memorial Bridge would receive oak grove and aesthetic treatments similar to those at the I-370 bridge but would also receive new fencing, additional lighting, "outrigger walks" to separate pedestrians from vehicles.

Phase 7: This project is a continuation of Phase 1 and of the state highway improvements at the northern end of the corridor. It would include replacing the concrete median with a new planting bed that has brick edging and groundcover plantings and new deciduous trees.



Phase One Implementation Strategy Add bulb-outs and neighborhood entry markers to residential side streets. Side streets planted with Quercus macrocarpa at 36'-0" on center. Pedestrian Streetlights. Frederick Avenue Quercus roba dat 40'-0" on center. Brick Paving Typical. -Liriope muscari groundcover plantings. MC BAIN AVENCE MCBain Avenue







looking east down Maryland Avenue

Implementation Strategies

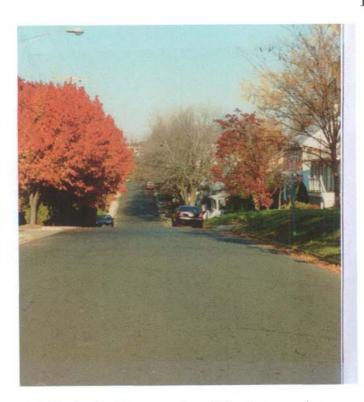
- 1. Remove sign with picture of truck.
- 2. Remove "No Thru Buses" sign (if this sign is to prevent Asbury buses from using this street, call and ask them not to).
- 3. Remove "No Parking" sign, paint curb if necessary.
- 4. Permit Parking past first curb cut.
- 5. Remove "Speed Checked By Radar" sign.
- 6. Remove "Do Not Enter" sign on left side of parking entrance.
- 7. Plant hedge to screen parking lot.
- 8. Move curb out, narrowing pavement, on both sides of the street near the intersection where parallel parking is not permitted. Install entrance piers see detail on page 37.
- 9. Plant new trees as per design code.



300 block looking north

Implementation Strategies

- 1. Remove "No Parking" sign.
- 2. Remove suicide lane sign (behind pole).
- 3. Plant a row of secondary trees on bank.
- 4. Move Fairgrounds sign to median.
- Remove a second "No Parking" sign (behind utility pole in front of Fairgrounds sign).



300 block, looking east down Montgomery Avenue

Implementation Strategies

- 1. Move curb out, narrowing pavement, on both sides of the street near the intersection where parallel parking is not permitted.
- 2. Install entrance piers see detail in the Design Code Application section.
- 3. Plant new trees as per Landscape Architectural Regulations.

Phase 1, Implementation Strategy

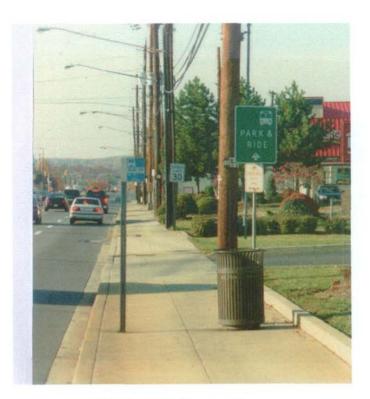


200 block north, looking north

Implementation Strategies

- 1. Widen sidewalk by ten feet.
- 2. Add street trees in grates per Landscape Architectural Regulations.
- 3. Move "Park and Ride" sign to median.
- 4. Install new "City" bus shelter with trash can, bench, and bike rack.
- 5. Plant row of secondary trees per design code.
- 6. Plant hedge to screen parking lot.
- 7. Encourage existing sign to be replaced with sign identifying *Shopping Center* per Environmental Graphics Regulations.
- Narrow parking lot entrance to two lanes. Right-in, rightout; no left turn due to new median.
- 9. Extend building to back of new sidewalk. (Ten feet inside of right-of-way).

Phase 1, Implementation Strategy



400 block looking north

Implementation Strategies

- 1. Widen sidewalk by ten feet.
- 2. Add street trees in grates per Landscape Architectural Regulations.
- 3. Move "Park and Ride" sign to median.4. Remove "No Parking" sign.
- 5. Narrow parking lot entrance to two lanes. Right-in, right-out; no left turn due to new median.
- 6. Extend building to back of new sidewalk. (Ten feet inside of right-of-way.)
- 7. Mount "Speed Limit" sign to utility pole.
- 8. Remove chain link fence.



Implementation Strategies

- Widen sidewalk by ten feet.
- 2. Add street trees in grates per Landscape Architectural Regulations.
- Remove "No Parking" sign.
 Remove suicide lane sign (behind pole).
- 5. Remove "Enter" sign to Popeye's.
- 6. Remove second "No Parking" sign in front of Popeye's.





200 block looking south

Implementation Strategies

- Remove truck sign (sign appears at right of curb describing a left-turn).
- 2. Mount "Right Turn" sign on utility pole.
- 3. Remove empty post.



200 block, looking south

Implementation Strategies

- 1. Replace existing landscaping of grasses with a more "urban landscape". Landscape should not block sign.
- 2. Permit outdoor seating with umbrella tables or useful awning between *Kenny Rodgers* and the sidewalk.



200 block north, looking south

Implementation Strategies

- 1. Mount "Right Turn" sign on utility pole.
- 2. Provide "City" bus shelter with trash can, bench, and bike rack.
- 3. Replace Fairgrounds sign with a new directional sign in median.

Phase 1, Implementation Strategy



200 block, looking south

Implementation Strategies

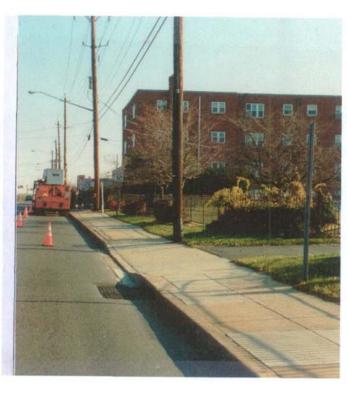
- 1. Remove "No Parking" sign.
- 2. Remove "Suicide Lane" sign.



200 block looking south

- Implementation Strategies

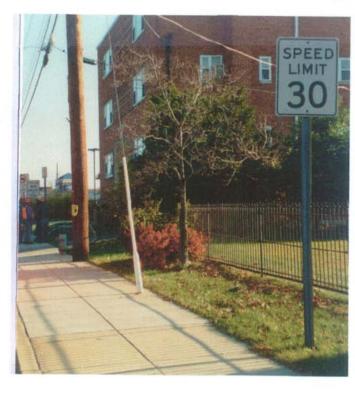
 1. Move phone booth to bus stop.
- Remove "No Parking" sign.
 See Landscape Architectural Regulations for treatment of Forest Oak Cemetery Frontage.
- 4. Move "AAA" and other small signs to a new pylon sign to reduce visual clutter when viewing cemetery.



200 block, looking south

Implementation Strategies

- 1. Remove empty post.
- See Design Code Application for concept sketch...

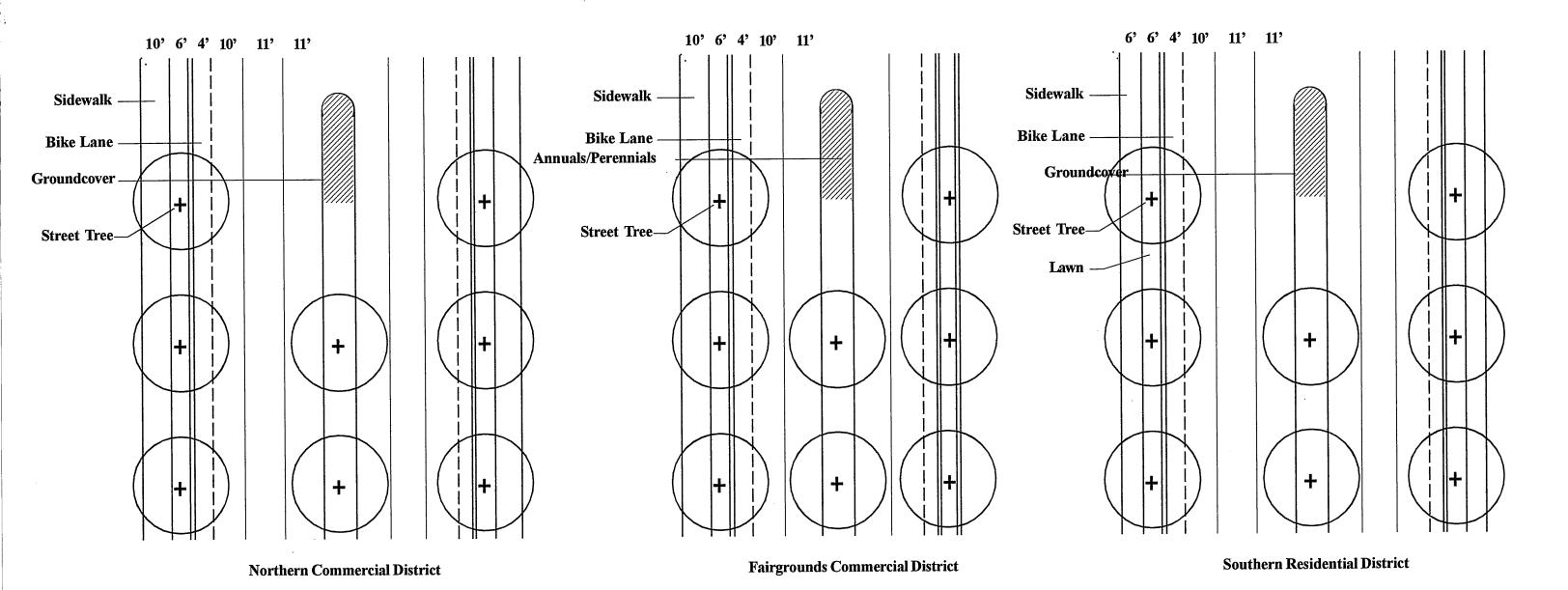


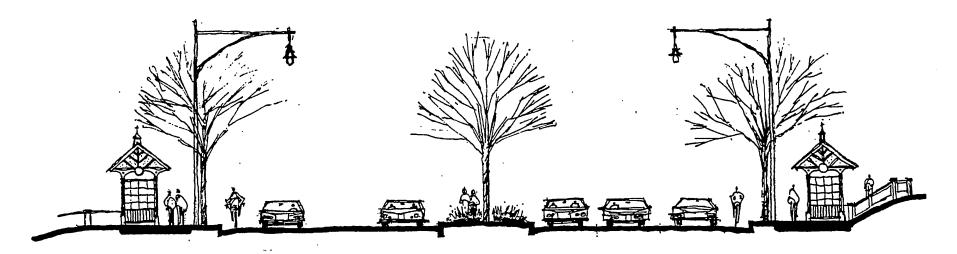
200 block north, looking south

<u>Implementation Strategies</u>

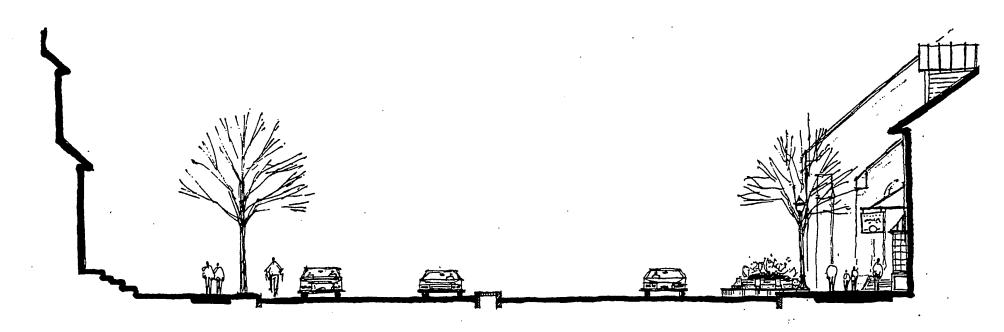
1. Mount speed limit sign on utility pole.

Thoroughfare Types

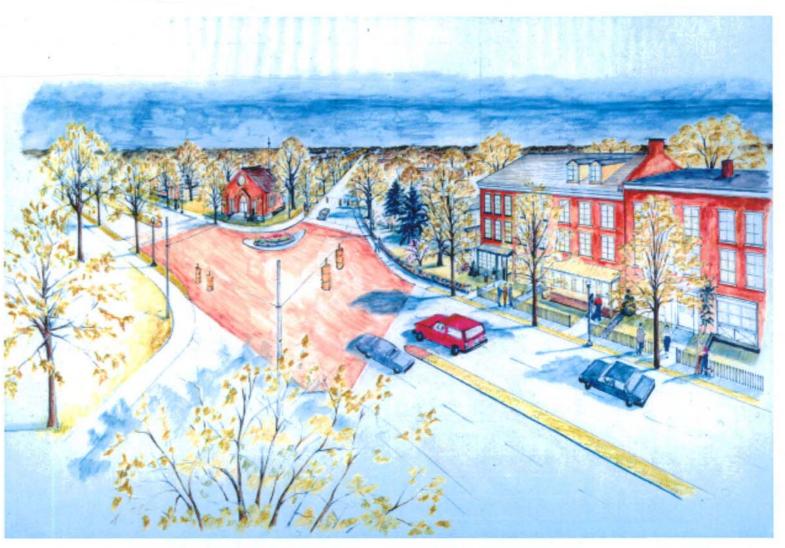




FREDERICK AVENUE AT EDUCATION BOULEVARD CROSS SECTION LOOKING NORTH



FREDERICK AVENUE AT SUMMIT AVENUE CROSS SECTION LOOKING NORTH



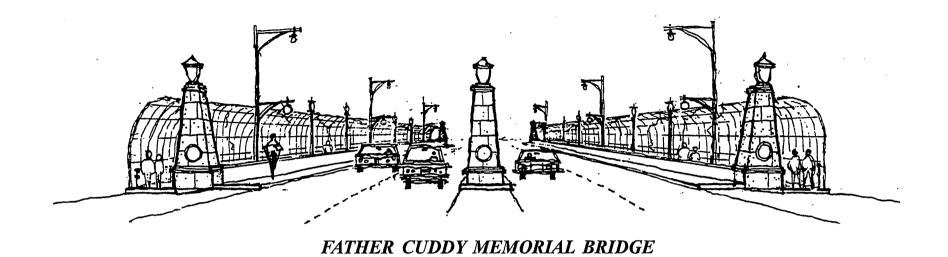
Frederick Avenue at South Summit Avenue

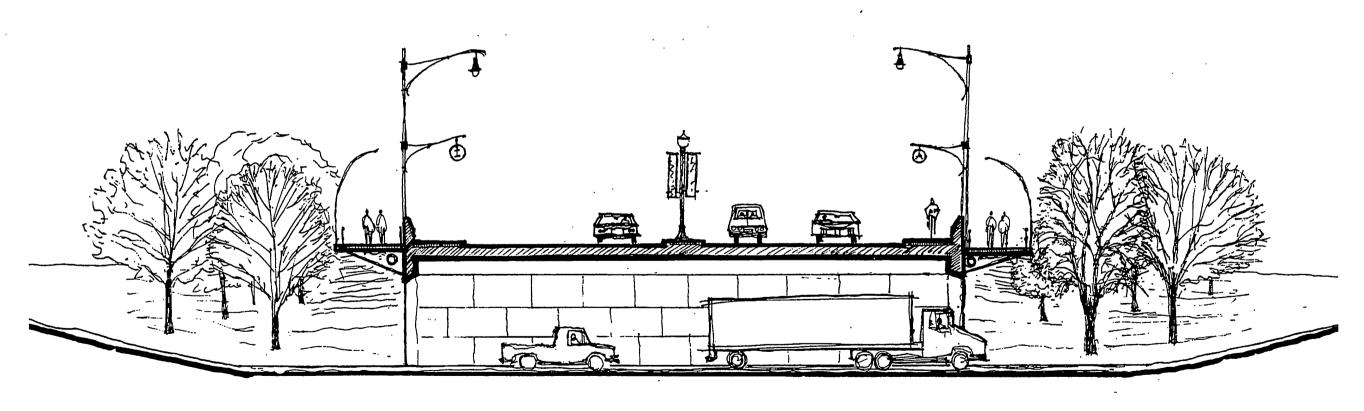


NEIGHBORHOOD ENTRANCE AT WALKER AVENUE

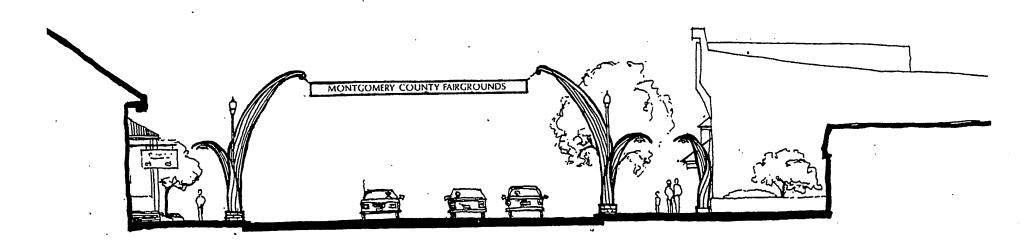


I-370 GATEWAY LOOKING NORTH

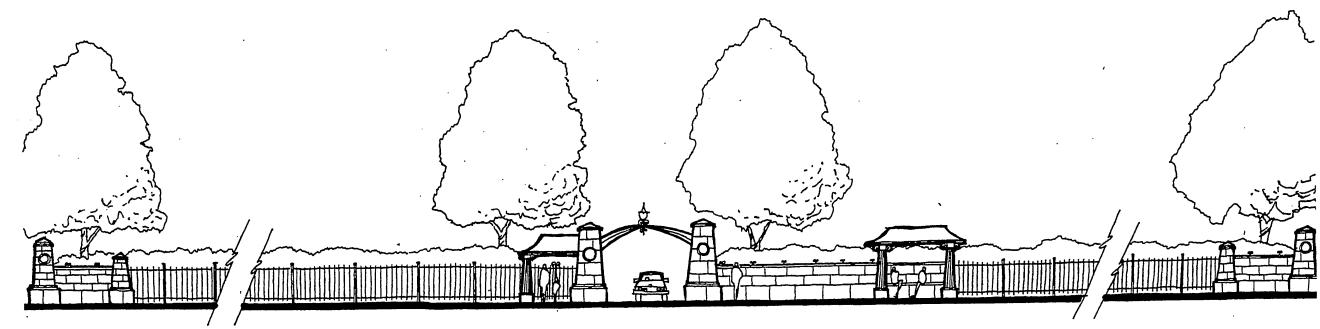




SECTION THROUGH FATHER CUDDY MEMORIAL BRIDGE



CHESTNUT STREET PROMENADE



FOREST OAK CEMETERY



Frederick Avenue at Walker Avenue
Looking North



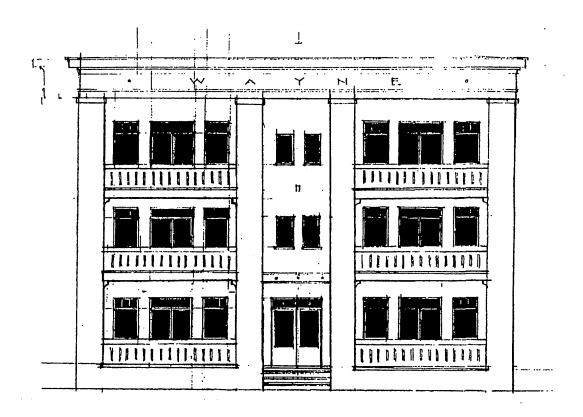
EXISTING FACADE NORTHEAST CORNER OF WALKER & FREDERICK AVENUES



PROPOSED FACADE NORTHEAST CORNER OF WALKER & FREDERICK AVENUES



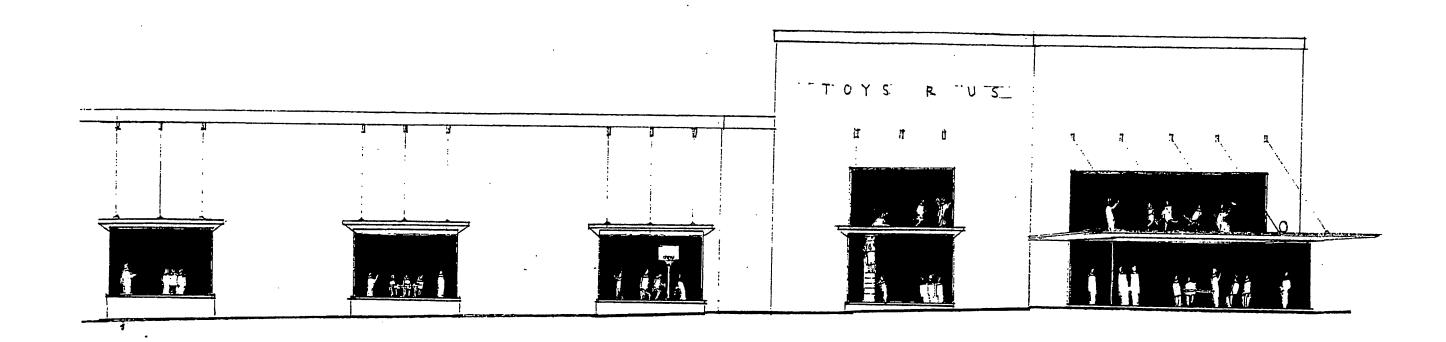
EXISTING APARTMENT FACADE
EAST DEER PARK DRIVE
& FREDERICK AVENUE



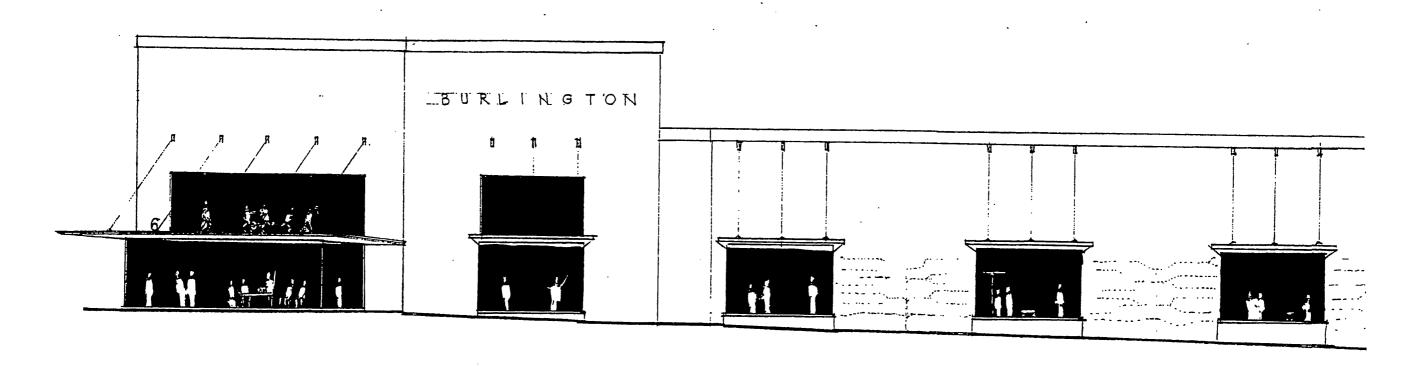
PROPOSED APARTMENT FACADE

EAST DEER PARK DRIVE

& FREDERICK AVENUE

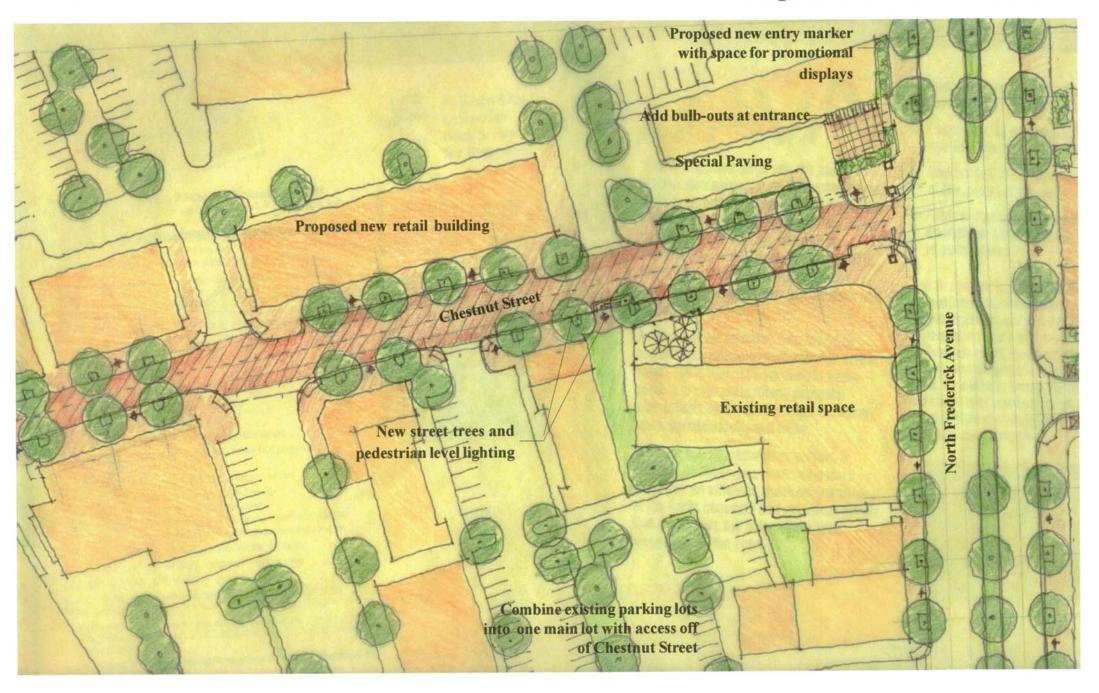


PROPOSED FACADE ARTICULATION
BURLINGTON COAT FACTORY
PERRY PARKWAY ELEVATION



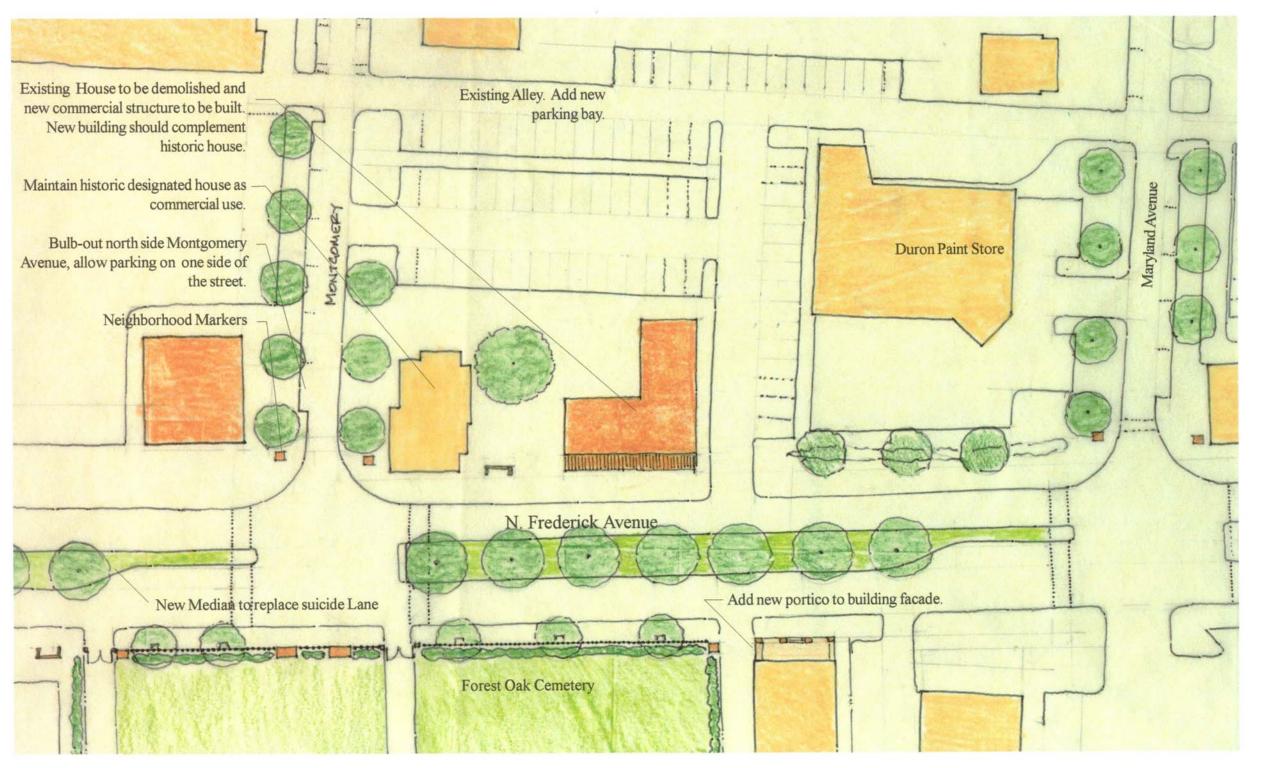
PROPOSED FACADE ARTICULATION
BURLINGTON COAT FACTORY
FREDERICK AVENUE ELEVATION

Neighborhood Transformations



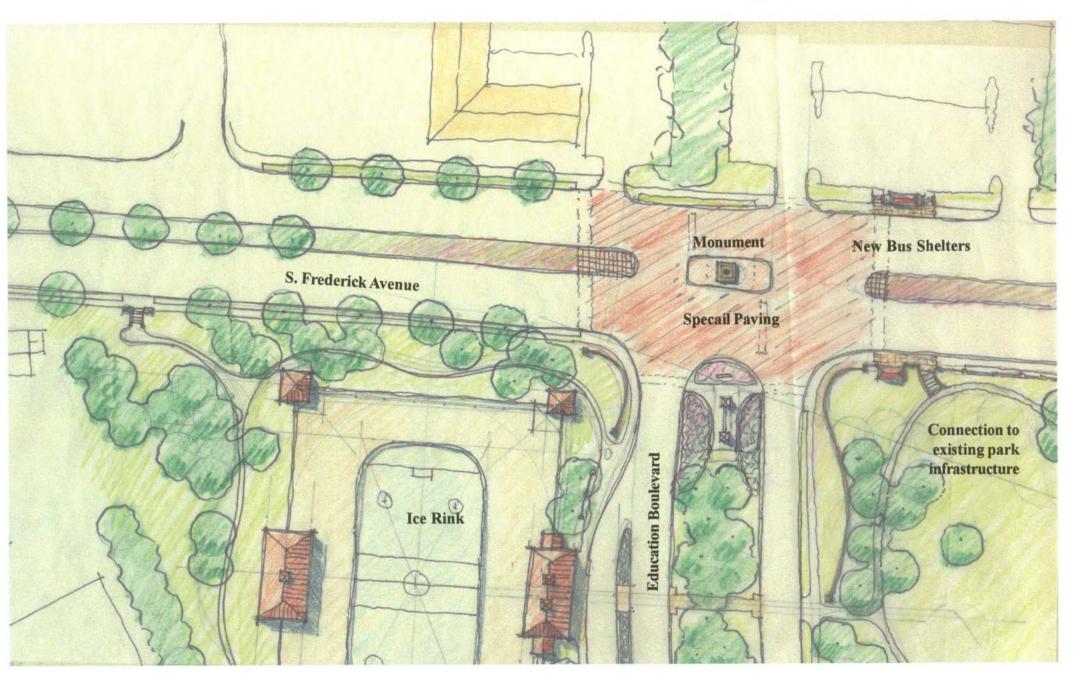
CHESTNUT STREET PROMENADE NOT TO SCALE

Neighborhood Transformations



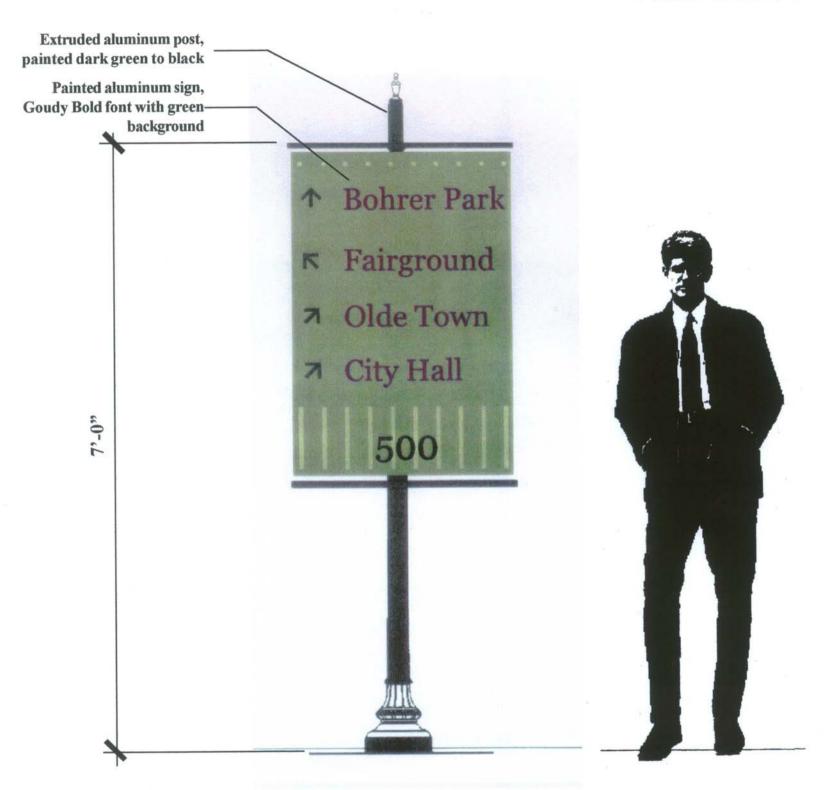
300 Block of North Frederick Avenue Parcel Redevelopment Sketch

Neighborhood Transformations



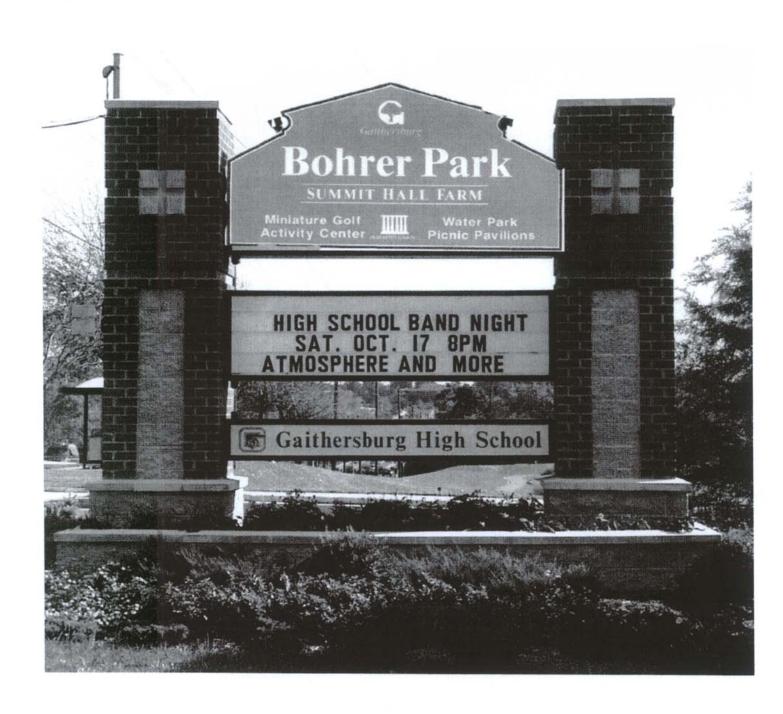
Bohrer Park at Summit Hall Frontage Sketch

Public Directional Signage

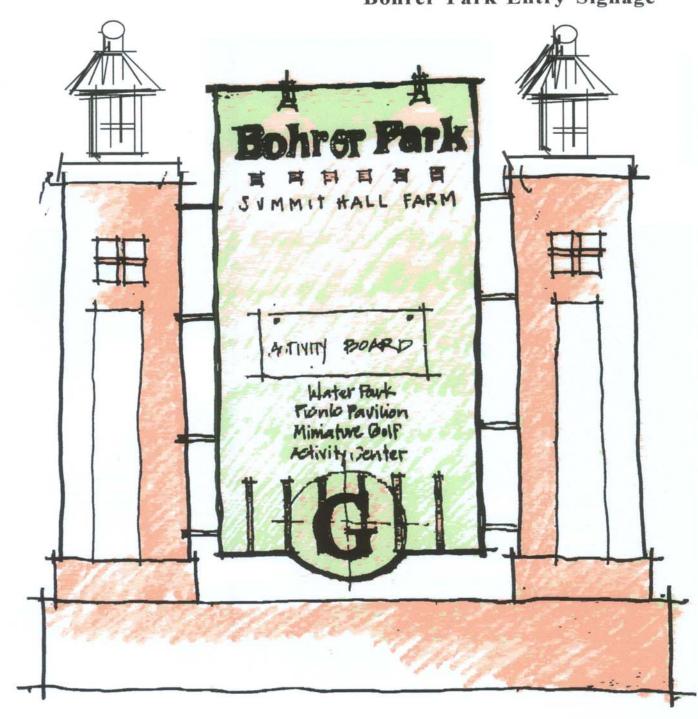


Conceptual Design Sketch for Directional Signage

Bohrer Park Entry Signage



Existing Park Entry Sign Modified with new Park Name

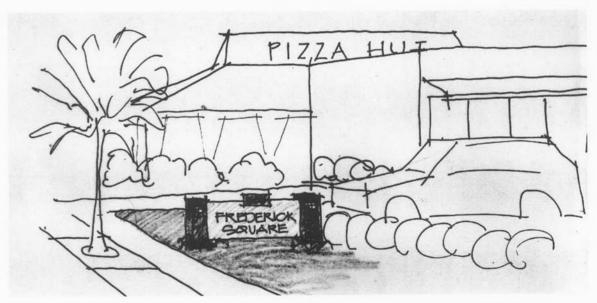


Proposed New Park Sign with downlighting.

Typical Monument Sign



Existing Pylon Sign



Application of the Environmental Graphics Regulations

Site Furniture



Proposed Trash Receptacle.
Recycling Receptacle to be the same with applied labels

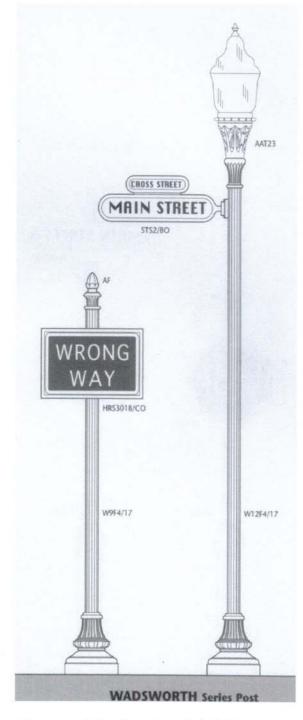


Proposed Metal Bench



ECO

Proposed Tree Grate



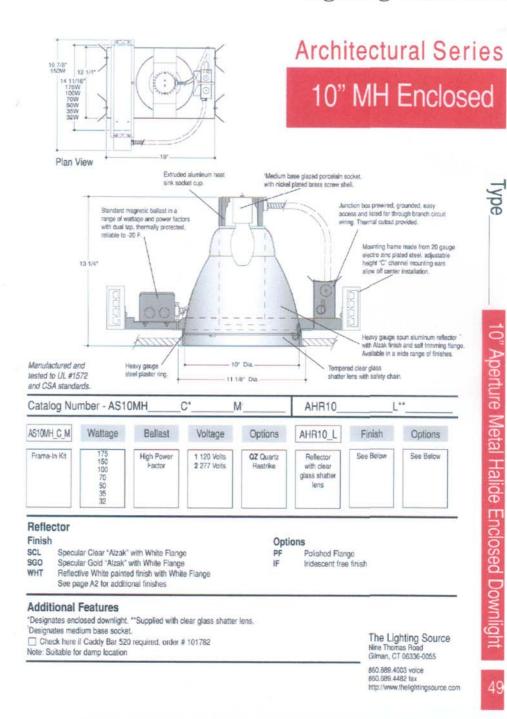
Proposed Pedestrian Lighting Pole and Fixture (Holophane Lighting Manufacturers)



Typical Shoe Box Parking Lot Fixture

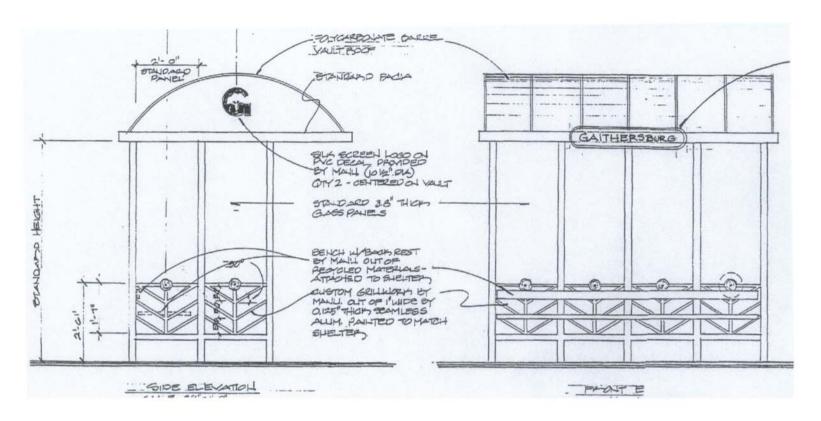


Proposed Cobra Head Fixture and Mounting Arm (Holophane Lighting Manufacturers)

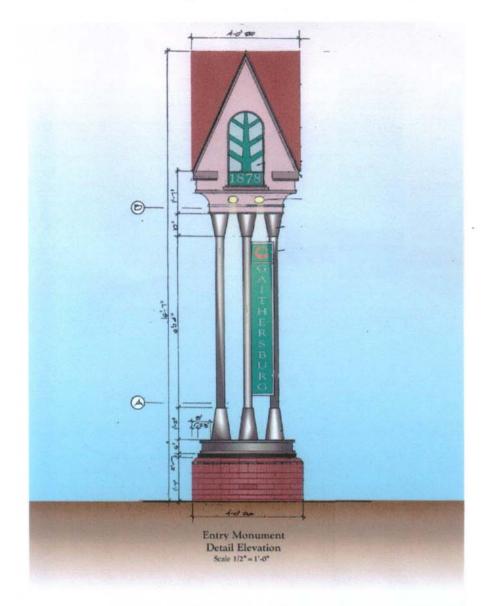


Typical Canopy Structure Downlight

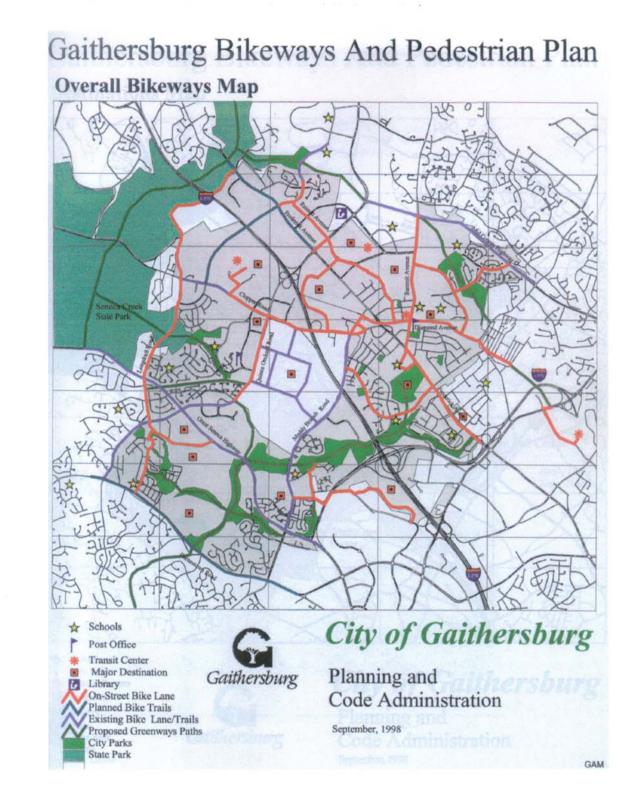
Entry Marker and Bus Shelter



City of Gaithersburg Bus Shelter



City of Gaithersburg Entry Marker



Proposed Intersection Improvements



MDOT Proposed Intersection Improvements at Montgoemery Village Avenue and N. Frederick Avenue (2 year time frame) Source: USR Greiner

Proposed Intersection Improvements



MDOT Proposed Intersection Improvements at Shady Grove Road and S. Frederick Avenue (2 year time frame)

Source: USR Greiner